

THE **DISPATCH**

MAY-JUNE 2017



**2017
NASG NATIONAL
CONVENTION**

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BILL'S BOILER WASH



By now, all of you should have received your election ballot. Please get them returned ASAP, as time to do so is running out.

The World's Greatest Hobby Show came to Phoenix in early February. This was a new experience for me. I spent several months getting the staff for the booth together. This was a very good thing, as I ended up in the ER the Friday of setup and was unable to help either Friday or Saturday, though I did spend all day Sunday at the show. I want to publicly thank those who took

care of business without my presence (it just goes to show how little I am needed — grin). At least one person that they talked to joined the NASG the following week. I also need to thank **Randy Bachmann**, the show's producer. He provides us space with all the power and tables, and also ships our basic booth materials with his own show materials at no cost

to the NASG. I will see to it that he gets special recognition from the NASG for his support. **Will Holt** has facilitated our relationship with Randy, so kudos to him as well.

August 8 through 12 is our annual convention in Timonium, Maryland, near Baltimore. Now is the time to get your contest entries together, not to mention your registration and hotel reservations! Sometimes things sell out if you wait until the last minute. You can see more about the convention by going to the NASG website, www.nasg.org, clicking on the **Events** tab and the selecting **Convention**. This will put you right into the convention's web site. I hope to see a bunch of you there. I was a bit under the weather at the last convention and missed seeing many of you then.

With the new BOT, we will need a couple of new committee chairs, the Contest Committee most certainly. If you care to participate in the NASG (I can recommend it — this is YOUR organization!), chairing or working on a committee is a good way to do this. Most committees require very little of your time, but like anything, the more you put into it, the more you and the NASG will get out of it.

Have fun! Bill Winans

MUSINGS FROM THE BEACH



By Monte Heppe
Executive Vice President

The time for this year's convention in Baltimore is fast approaching. It is shaping up to be a great one. The major tours are all excellent. Having lived in the Baltimore area for years, I have enjoyed all the venues a number of times. On the Strasburg tour, the **Pennsylvania Railroad Museum** is a gem featuring a number of PRR steam and electric locomotives and a variety of rolling stock,

which have been beautifully restored. It also has an excellent bookshop. The **Choo Choo Barn** display, unlike some others is constantly updated and improved and never looks dated. Adjacent to the Barn is the Strasburg Hobby Shop. The ride on the **Strasburg Railroad** meanders through beautiful scenery with a number of Amish farms and turning around at Paradise next to Amtrak's Harrisburg line. The railroad was incorporated on June 9th, 1832, making it the oldest US railroad continuously operated under its original charter and name.

Steam into History is an opportunity to ride behind a newly built 4-4-0 American locomotive. You will hear all about the history on board the train.

The **B&O Railroad Museum** is among the best in the country. It chronicles the history of the nation's first railroad from its magnificently restored roundhouse. The museum entrance is in the B&O's original station.

I will be involved in three clinics at the convention. The first is a panel discussing **tips for entering the model contest** with the NASG Model Contest Chairman and two former chairmen.

Next will be **Plan, Design and Operate a Switching Layout** presenting the basics of switching layout features and my experiences with my own switching layout, the Calabash Terminal Railroad. The CTRR has been featured in the Dispatch and the magazine of the Layout Design SIG. The CTRR will be on display at the convention.

Last is a joint presentation with **Charles Malinowski** on **T-Trak-S**, an interesting module concept featuring small lightweight, and easily transportable modules. While originally developed with N scale in mind, standards exist for all scales. For a preview, turn to page 12 of this issue of the Dispatch or go to www.t-trak.org

Be sure to stop by my layout and say Hi. Especially if you have any comments on what we can do to make the organization even better.

**DON'T FORGET TO
VOTE
YOUR BALLOT MUST BE
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THAN MAY 15, 2017**



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THE DISPATCH

MANIFEST

Volume 41, Number 3, May-June 2017

COVER On the left Tom Hawley's consist of mostly Iron Rails tank cars runs on the Southeastern Michigan S Gaugers' layout at the 2015 Spring S Spree while Jerry Poniatowski's Lionel-American Flyer New Haven passenger set is on the right.

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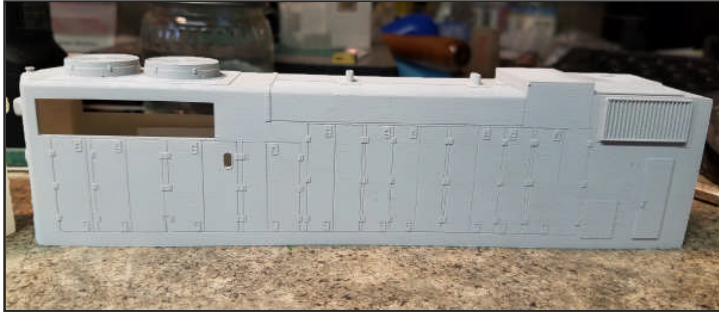
NASG Web Site

<http://www.nasg.org>

NASG Website Director, Peter Vanvliet: webmaster@nasg.org

PRODUCTS

3D Printed Loco Shells

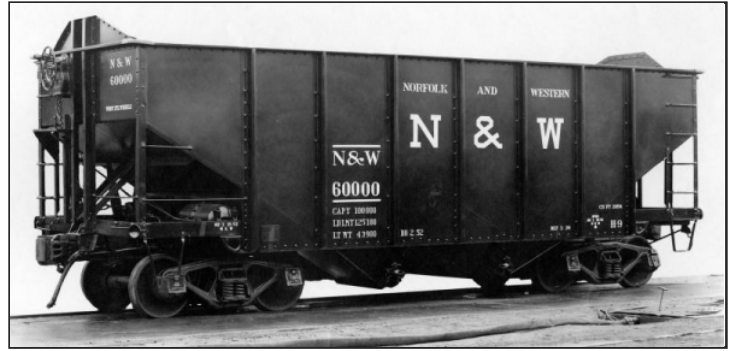


Announcing that **Century Models** is producing 3D printed parts in S scale. Many parts are available for printing and there are many more to come. Among the parts available are GP38-2 long hoods in both dynamic and non-dynamic versions, EMD dash-2 cabs with the 81" nose, fuel tank ends (allows you to model any size EMD fuel tank with that profile), EMD locomotive pilots both notched and non-notched, radiator fans, grills and much more.

Robert Frascella and Michael Ostertag are developing detail parts that can be 3D printed to fit many second generation EMD diesel locomotives. There will be many more parts to come in the future, including Alco C420, EMD GP39-2, GP40 and 40-2. Possible conversion kits to convert an American Models GP35 into an EMD GP30 (very far down the line, but in the works) also a potential MP15AC.

They are also working on parts that will allow a modeler to build a PS 4750 3-bay covered hopper. This project is also down the line a bit, but is being worked on.

All of these parts will require the modeler to build their own chassis using strip styrene, but the end results are incredible! So check out the Century Models Store at <https://www.shapeways.com/shops/century-models?li=pb>



Great Decals offers Hopper set

Decals for Norfolk and Western class H-9 and H L 2-bay hopper cars (set #138), in white, are available in S-scale from Great Decals, 3306 Parkside Terrace, Fairfax, VA for \$7.75 per set, postpaid. Virginia residents please include sales tax.

Each set provides 12 canned road numbers, so the modeler can easily make any road number valid for the cars. Each set does one car. The decals allow modelers to produce many uniquely numbered, scale N&W 2-bay hopper cars like the prototype pictured above.

These decal sets include the 24" N&W herald, road name, road numbers for both sides and ends, N&W car classes, repack stencils, dimensional and end data specific to these cars. Alternate LT WT lines are also provided.

Visit their web site, www.greatdecals.com to see their large collection of available decals.

monster modelworks S SCALE Brick Yard Tower

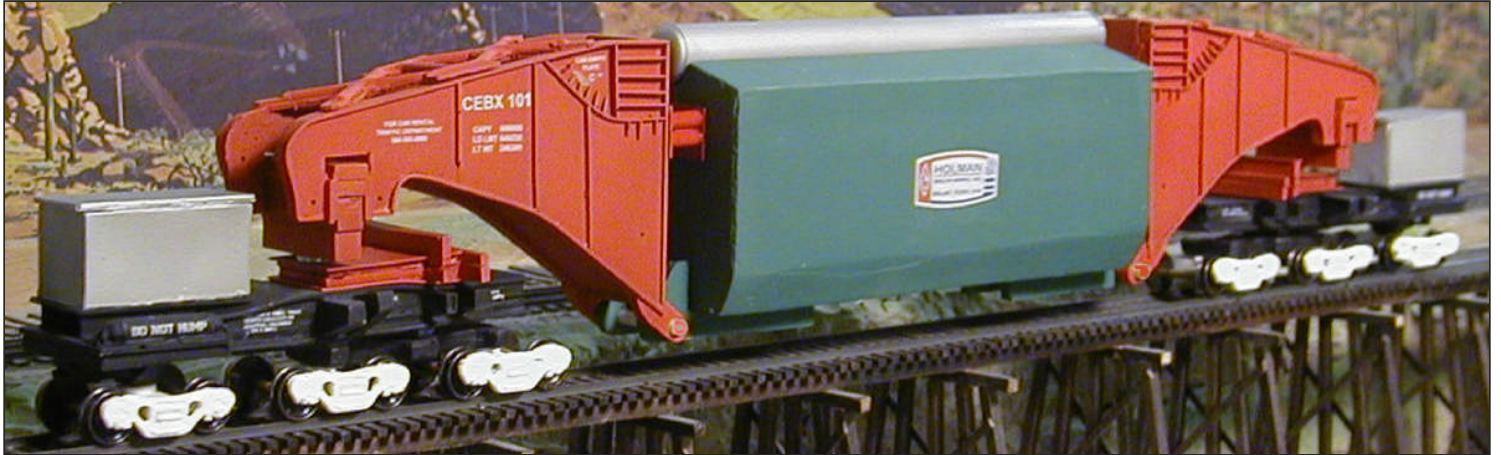
Based on the yard tower in East Deerfield, Massachusetts. This tower has three sister towers located on the east coast.

INCLUDES: 3D Laser engraved American Bond brick walls.
3D Laser engraved brick corner pieces • Simulated rain gutter
3D Laser engraved brick underlays • 3D engraved old chimney
3D Printed chimney pipe • Decorative brick cornice overlays
Lasercut concrete overlays • Diamond cut asphalt shingles
Lasercut peel and stick windows with glazing • 3D Printed stairs
DIMENSIONS: 3.25" wide x 5.2" deep x 5.45" high.

For more information and to order go to:
www.monstermodelworks.com



PRODUCTS



CONCEPT MODELS CEBX 101 SCHNABEL CAR KIT

DAVID ALLEN of **CONCEPT MODELS** has announced that they are producing the CEBX 101 Schnabel car in S-scale. Schnabel cars are specialized freight cars made for carrying extremely heavy or oversized loads. The end structures have a massive appearance, making this a very unique car, running with a load or empty. The price for this kit is **\$99.99 plus shipping**. This kit is sold without load, trucks or couplers. A truck frame kit molded in white resin is available.

The 6-truck design should function well on standard S Scale layouts with standard curves.

The model consists of resin castings and assembly hardware. Hand grabs, ladders, and related metal details are not provided. Focus is on providing the basis for modelers to exercise their skills in taking the model to the detail level desired. Instructions are all photo illustrated by an experienced technical writer.

In addition to the car kit, two optional loads are available; a boiler load at \$49.99 and a pressure vessel load priced at \$69.99.

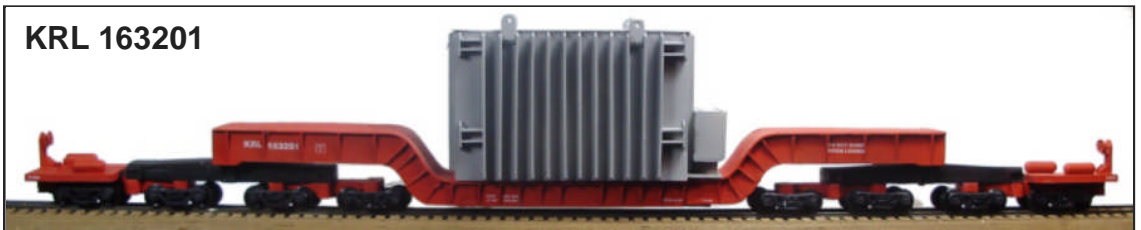
The company plans to release their entire line of depressed flat and Schnabel cars in the future. Specific release dates will be influenced by customer interest. Pictured on the right are some of the cars that are available in HO scale that will become available in S scale if there is sufficient interest.

Concept Models currently markets a variety of models available at the web site <http://www.con-sys.com>

CPOX 820



KRL 163201



GPUX 100



KRL 204002



ARMCHAIR MODEL RAILROADING

A Good Idea? Sometimes!

By Jeff Madden, Editor Emeritus

Oh sure, there's a negative image of those who *armchair model railroad* a lot, supposedly wasting valuable time that could be spent for actual physical model railroading.

But, I think there are some positives to *armchairing*: It can be relaxing, inspirational, educational, and might even be a necessity. By necessity I mean delving into "how to" articles when the time is right or for keeping up with current trends and technologies.

Let's examine armchair model railroading in a journalistic manner: **Who, what, when where and why and how.**

Who: Who should consider *armchairing* in our hobby? Of course, those without a home layout or the space to build one. Maybe those constantly on the move like those in the military, folks who are new to the hobby — both youngsters and adults just taking up the hobby (they need the education). Modelers looking to progress in certain aspects of their own layout building — scenery, wiring, benchwork, track design, etc., and finally, those modelers wishing to change scales.

What is it: Well, *armchairing* is simply that — sitting in an easy chair and reading model and/or real railroad magazines and books. You might want to read new issues of mags that come in the mail or you picked up at the hobby shop. You might want to read back issues of magazines from your own collection, or real train books from the bookshelf, or even various "how-to" softcover magazines from your own stash.

When: When is similar to the where, but here we're concerned with the time frame to *armchair*. My favorite time is when I'm watching TV. I might grab a stack of old Model Railroaders, Dispatches, S Gaugians, RMC's, etc., and thumb through them during TV lulls — commercials, time outs, etc. You can practically read a novel during those long and tedious health care commercials. When TV watching I usually pick a stack of "like" magazines and thumb through them from oldest to new. Sometimes it's Heralds from 1960 to 1977, sometimes it's MRs from the 1930s. No, I don't pile up all the years at once on the coffee table, just maybe a year or two at a time.

Where: Hmm, places where you might *armchair*? Well, the term itself suggests sitting in an easy chair, but it might also mean lying in bed at home or sitting in the *library*. Or it could also mean taking along a stack of reading material when away from home such as when in motels, visiting relatives or traveling on public transportation. The modern way when away from home might be to use technology such as a laptop or an I-phone. For example, the complete Model Railroader archive DVD can be popped into a laptop. Many of you might prefer just to grab a stack of mags and throw them in a briefcase. Don't forget the *waiting places* as good locations to catch up on your reading — waiting rooms in doctors' offices or waiting in the car while the little woman does some shopping.

Why: There are several usual reasons to *armchair*. One might be just to review various issues or books you haven't looked at for a while. Maybe it's just for pure inspiration. But it just as easily could be to research a particular topic that would fit

your current modeling efforts like bridges, scenery, paint schemes, layout design, benchwork, track laying, structure plans, car plans, etc. Maybe you want to refresh yourself on the history of the hobby or of a prototype railroad. Remember too that even articles or mags or books on scales other than S can give you ideas for your own use. I've found 2-rail O scale publications in particular have articles and ideas much in common with our situations in S.


How: When perusing back issues especially, don't read the entire magazine or book word for word. Learn to scan by seeking article titles or key photos that follow a theme or purpose for what might be on your mind at the time.

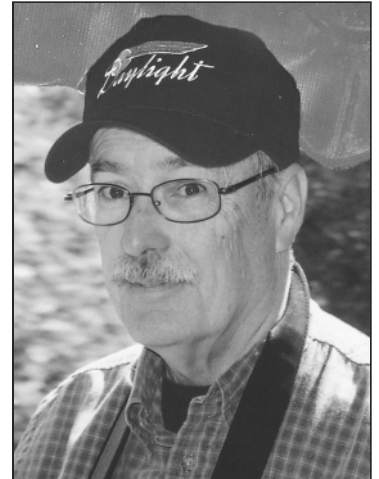
Which: Here's one I almost forgot. Which hard copy publications should I have on my *must read* list, either current or back issues? If you're in S, I'd say the **Dispatch** and the **S Gaugian**, both bi-monthly. If you're in S narrow gauge read **Sn3 Modeler** and the **Narrow Gauge & Shortline Gazette**. Other model rail pubs should be of occasional interest and these would be current issues of Model Railroader, Classic Toy Trains, RMC, Model RR News, O Gauge Railroading, O Scale Trains, NMRA Magazine, TCA Quarterly, and maybe some more. I highly recommend Great Model Railroads and Model Railroad Planning annuals for inspiration in any scale. Read at least one real train magazine: Trains, Classic Trains, Railfan. If you have a prototype preference, read at least one historical publication.

My own *must read* list of current pubs include the Dispatch, S Gaugian, MR Planning, GMR, Classic Trains. Having accumulated many model and real train pubs over the years, I have a substantial stash that I can always go over (and sometimes weed out). I get the NMRA Magazine to support that organization, but I feel that it only needs to be printed 4 or 6 times a year. I subscribe to O Scale Trains because of the similarities with S, and I get the quarterly magazine from the Baltimore & Ohio Railroad Historical Society.

So there you have the case for *armchairing*. To me, TV commercials offer the best excuse for looking over your hobby magazine stash without feeling too guilty at not actually working on your layout or building a model. Don't let *armchair model railroading* take over all your spare time! Do it creatively and in a timely manner. Let your *armchairing* push you into action even it means working on a club layout or a friend's.

While you're at it, don't forget to weed out back issues, books, catalogs that are no longer of interest or relevant to your current modeling. You can sell back issues (recommend bundle by year), books, etc., at the swap meets. You can give away recent publications to libraries, doctors offices, etc.

In conclusion — make your *armchair* reading count and do it to take advantage of the lulls that you encounter when doing sedentary activities. 



2017 NASG CONVENTION

Register Now! Don't miss out.

It is hard to believe that the convention is just four months away. Preparation is underway in to insure that this will be a great convention. If you have not sent in your registration along with the tours you would like to take, do not wait. Requests for the tours must be submitted no later than July 7th. **The tours will not be available for purchase at the convention.**

Vendors: we want to meet your requirements for tables, so now is the time to send in your registration.

Convention Cars: The 2017 B&O TOFC is going fast. If you want to secure one, do not wait until August as they may be gone and once they are gone, they are gone.

The members of the Baltimore Area American Flyer Club and the Washington and Old Dominion Club are excited about this convention. Updates are posted frequently on the club Web site at <http://baltimoreamericanflyerclub.org/home-desktop-2/2017-nasg-convention>. Convention forms, hotel information, updates, convention schedule, and clinic information can all be found on this link. Visit often to be sure that you have the latest information. I am looking forward to seeing you all in August. — *David Blair, Convention Chair*

Clinics at the 2017 Convention:

Many consider the Clinics to be the highlights of any model railroad convention. Following is a list of clinics that have been confirmed for the coming convention.

Plan, Design and Operate a Switching Layout: by Monte Heppe

Wednesday 6-7 pm, Chesapeake Room 1

Presenting the basics of switching layout features and Monte's experiences with his layout, the Calabash Terminal Railroad.

How to Enter a Model in the Model Contest: by Monte Heppe and Jeff English

Wednesday 8-9 pm, Chesapeake Room 1

It is important to know how to enter your model in the NASG model contest including filling out the forms so that your model can be properly evaluated. This is an opportunity to learn from past model contest winners how to properly enter your model.

Legacy 101 for S Gauge by David Horn Wednesday 7-8 pm, Chesapeake Room 2

Have you been hesitant to buy a Legacy engine because you are not sure how to make it work? David Horn will take you through the basics on how to set up an engine and how to use the Cab 2 Controller.

Make and Take by Ken Zieska Wednesday 9-10 pm, Chesapeake Room 2 Thursday 9-10 pm, Chesapeake Room 2

Ken will provide instruction on building the Monster engineer building. This kit can be used on your layout as a stand-a-lone or as a head building on another structure. There will be a fee for participating to cover the cost of the kit. Be sure to bring your model building toolkit as Ken will have limited tools available. This kit has a lot of potential for use on your layout.

<http://monstermodelworks.com/S-Scale/S-Kits/s-scale-brick-engineerstoostools-shed.html>

This year's **Greeley's Place Contest** has the common theme of **Plasticville**. Rather than have everyone start with a specific kit, you can start with any Plasticville kit. Bring your completed kit to the contest, and earn a \$5 entry prize. In addition, there will be cash prizes in at least two categories. One will be **Original Build** where you build the kit out of the box and add paint and details of your choice to enhance it. A second category will be **Kitbashed**, where you make significant modifications to the kit, or combine it with other kits, either from Plasticville or another manufacturer. Depending on the number and creativity of the entries, there may be other categories. Our panel of esteemed judges will distribute the prizes. To be eligible, the building must be a separate structure displayed in the model room. You cannot claim a building on a portable layout to be your entry. You may enter several models, but you are only eligible for one \$5 entry prize.

How to Modify American Flyer Switches by David Avedesian

Thursday 6-7 pm, Chesapeake Room 1

David will discuss how to modify American Flyer switches for flawless operation into a yard that represents the actual spacing and operation of a real world railroad yard. You will be instructed on the way to modify the switches and track on Wednesday and then be able to visit David's layout to see it in action on Friday.

Getting the passenger cars you want in S Scale by Jamie Bothwell

Thursday 7-8 pm Chesapeake Room 2

Jamie will show you some of the techniques he has used to improve the quality and variety of his passenger car fleet. Planned topics include laser cut sides for lightweight cars, and reconfiguring American Models heavyweights to get other car types. He will also cover reference materials, decals, available detail parts, and folding those pesky paper diaphragms. Some underframe detailing will be covered if time allows.

How to Repair American Flyer Steam Engines by Tom Barker

Thursday 8-9 pm, Chesapeake Room 1

Tom Barker will provide detailed instructions on how to repair your American Flyer steamers and get them running again. Limited quantities of his book will be available to help you become experts on maintenance of your steamers.

Installing People in American Model Passenger Cars by William Novak

Friday 4-5 PM Chesapeake 1

Many recent products entering the S Gauge market have emphasized a greater attention to details that adds to the allure of the gauge to new and seasoned operators and collectors alike. The American Models Budd passenger car is a prime example of rolling stock that has both exterior and interior details not usually found in other S Gauge passenger cars and which also offer an opportunity to add even more detail. In this clinic Bill Novak will show how to select, prepare and install a complement of seated passengers to the existing seating in a Budd car adding even more detail and interest.

Continued on page 23

NEWS FROM THE NORTH

By Jim Martin, S Scale Workshop

A Canadian Marriage:

Citing a “strong alignment of culture and values,” two premiere Canadian companies are tying the knot. In February, **Tim Warris**, founder of **Fast Tracks Hobbyworks** announced his intention to purchase **Mt. Albert Scale Lumber**.

Fast Tracks has become world renowned for its vast array of innovative track building products, while Mt. Albert has a similarly commanding reputation for high quality scale lumber. Tim says it’s an opportunity for both brands to “achieve significant growth in their primary markets.”



Mt. Albert and its subsidiary, Mt. Albert Scale Models, will retain their names and product lines but will be integrated into the Fast Tracks e-commerce system. Mt. Albert will also move its plant and headquarters from the Hamilton area to an expansive new facility in Port Dover, Ontario, which is the current home to Fast Tracks. Jerry Cornwell of Mt. Albert will continue to develop new kits and represent his former company at various shows. Good luck and our continuing thanks to both men for their contributions to building in 3/16” scale.

Work Areas: Three of my S-scaler friends have been improving their workshops. It doesn’t matter whether you are a chef, a mechanic, or for that matter, a model railroader, a clean, organized and well-lit space not only improves one’s work, it’s much more enjoyable to be in.

Trevor Marshall has been blogging (Port Rowan in 1:64) about his new workshop. While extensive renovations on his Toronto home were underway, it just made sense to upgrade his workshop at the same time. Check out the photo below.



The L-shaped cupboard and drawers are from IKEA. They are topped with two layers of three-quarter inch plywood. The top surface is cork tile. That may not sound practical, but this is not a work surface. Instead, it’s a platform for various power

tools, and a place to marshal tools and projects. The ledge along the back will hold test tracks in various scales and gauges. There is huge drawer space, including some that are 36 inches wide for storing Styrene and strip wood.

Out front is a Festool MFT/3 free standing work table. This is movable but very sturdy with dog clamps for holding larger projects. A home-made Masonite top covers the dog holes and converts the table to a modeling bench.

Trevor has installed home-made bars along the shop ceiling for holding LED photo lights on adjustable arms, for illuminating projects or taking pictures. Among the tools are a Sherline Mill, a resistance soldering unit, sander, chop saw, modeler’s drill press, a Flex-Shaft tool, and more. He also has a vented booth in an adjacent room for exhausting nasty paint and adhesive fumes. Some adjustable height bar stools permit various levels of work comfort. Needless to say, Trevor really appreciates his new work surroundings.

Simon Parent recently moved to a new home in the Montreal Suburbs. With early retirement approaching, he is looking ahead to doing private, professional model building. Therefore, a functional and comfortable workshop is not a luxury, it’s a necessity. Work on his shop progresses, but check out that gleaming work surface in the photo below.



The stainless steel bench top looks like it belongs in a high end kitchen, but will easily shrug off any future paint spills or solder drips. He bought it from Home Depot and mounted it on IKEA adjustable legs so he can sit or stand at his work. Beside that is an exquisite, hand-built travelling tool case that even houses a DCC power supply for train shows. Ventilation is of course a given for both his paint booth and his laser cutter.

This is all happening in a studio above his garage. Skylights and windows provide a lot of natural light in the daytime. The garage below houses Simon’s woodworking shop where he does layout benchwork and module construction.

David Clubine of Ridgehill Scale models in Brantford, Ontario has taken some time out from layout construction to improve his work surroundings. David has a sharp eye for a bargain and scooped up some discontinued IKEA cabinetry at a great price. It is topped with cork from the dollar store. Its center section has an easily-replaced hardboard surface. The two corner cupboards house a small lathe, jig saw, belt sander, strip wood and styrene tubes. It supports a yet-to-be-vented



paint booth, soldering station, vise, and a drill press. It's also a place for a lovely wood tool chest inherited from his father. Storage capacity will increase when David installs a pegboard partition between the workbench and the house utilities.

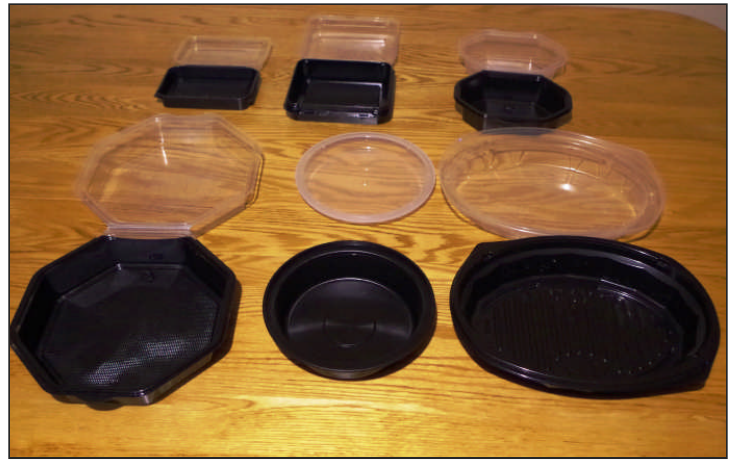
Workshop Envy: Some of you may be awed by these work places, but building one is no different than building a layout. Take the first step and then budget some hobby time and money toward your future enjoyment. In the end you want decent lighting and ventilation, good storage and elbow room.

These three guys all have something in common: each is at least 25 years younger than me. I know that if I had built a decent workshop when I was their age, I would no doubt have more to show for my years in the hobby. At my current age I'll probably just plod ahead with what I have. Just to make you feel good about yourself, I offer a photo of my shambolic working conditions below. The "project" and other props on the desk, are simply me acting the fool, but the rest of it is for real.



Seriously though, there are many inexpensive ways to organize one's workshop. Here's one.

Ken Garber of Windsor, Ontario wrote to me recently reminding me that the take-out containers from a well-known Canadian restaurant chain make for practical and elegant-looking project storage. In his words: "In Canada we have a chain of restaurants called Swiss Chalet. They're known for their chicken and ribs. When you order take-out, the meals come in plastic snap-lid containers in picture above right.



Once they're washed out (you don't want BBQ sauce on your model), they make great little containers. The current ones are about 9.5 inches long by 6.5 inches wide and about 4 inches high with the clear lid on. They do change the style from time to time; a previous container was about 8 inches in diameter (hexagonally shaped) and about 3 inches high with attached clear lid; ample for an SHS switcher and its bits and pieces as seen below.



They have another container for apple pie or salads; this one 6 x 4 x 3.5 an with attached clear lid. They come free with the food of course, and should be good for small projects or the storage of ballast and the like."

Thanks Ken, I'll have that SW-1 cut into quarters to go. Ken, who cuts a manly figure, is obviously well schooled in take out. I can vouch for the rectangular, one-piece containers with the black bottoms and clear lids. They look posh and stack well. I use mine for storing foliage and other scenery materials. Check your own local takeout establishments for useful shop containers. ~~BTW~~

Till next time, Cheers, eh.



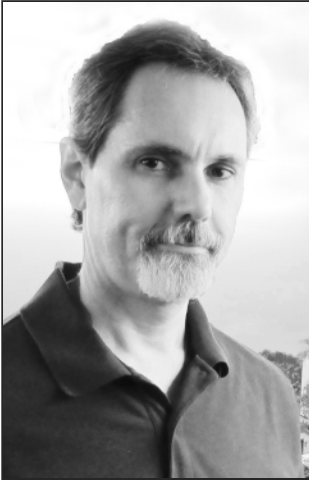
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THE NASG — WHO ARE WE?

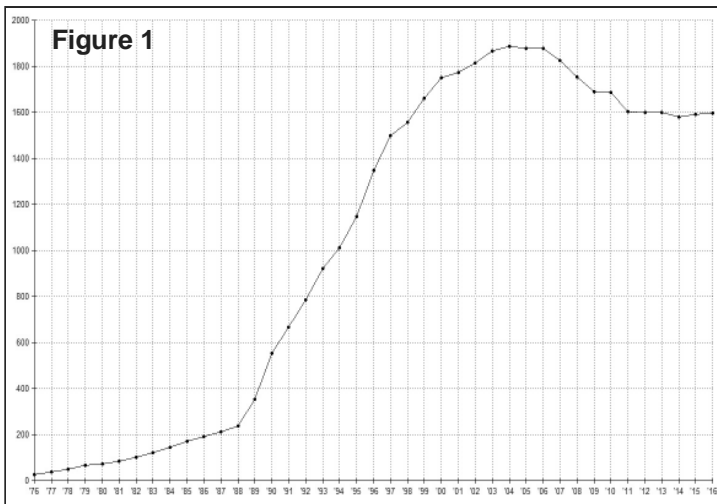


By Peter Vanvliet
NASG Webmaster

Don DeWitt wrote an article in the October, 1989 issue of The Dispatch, titled **The NASG, Who Are We?** It is about who makes up the NASG membership. While scanning all of the Dispatch issues into PDF and making them available on the NASG web site, I spent some time reading through a lot of them. This has been a crash-course for me, getting up to speed on the world of S as I had switched from N-scale to S-scale in 2008. What I find interesting is reading the commentary

from throughout the years in various magazines (not just the Dispatch) stating such things as “the doom of S,” the decline of available products, and the decline of the hobby itself. Yet, here we are in 2017. The amount of product available in S is quite staggering. There are 383 companies listed in the **S Resources** section of the **NASG web site**. There are large numbers of visits to the NASG web site, considering our niche market, and a steady membership head-count in the NASG organization itself.

It is this last fact that I want to delve into further, and to make it a bit of a follow-up on Don DeWitt’s article; to see where we stand 28 years later. Don wrote, “As of September 10, 1989, the total paid membership of the NASG is 862.” As of February 2017 the total paid membership is 1,603. Nearly double! See figure 1 below for a historical chart that uses data provided by the former NASG Treasurer **Jim Kindraka**, and current Membership Secretary **Dick Kramer**. When I took over the NASG web site, I added the **Membership Statistics** page to the list of things you can view when you log into the **Members Only** section of the web site (see **NASG** button on the web site). This page does a number of hard-wired database searches within our NASG database, so that it gives you the most up-to-date information. The first time that I ran that page, the membership count was 1,604. Needless to say, it fluctuates a bit, but every time I have looked at that number, it has never varied



more than 20 plus-or-minus. So 1,600 is about where our membership has been for the past five years. This is despite the fact that we are losing some of our World War II-generation members due to old age. That shows that younger people must be joining the organization!

Don’s article was based on feedback he got from a survey he had included in an earlier 1989 Dispatch. Thanks to the membership records we now keep, we can get some of that information directly from our database. He reported that 43% of the membership were scale modelers (standard and narrow-gauge), and 40% were American Flyer enthusiasts, with 17% indicating that they enjoyed both. Since that time we have created the hi-rail group, which are people who enjoy more-to-scale modeling, but still run AF equipment and have some AF accessories on their layout, while attempting to make their scenery a bit more realistic. Given that, today we have 71% of the membership enjoying AF, 54% flagging themselves as hi-rail, and 43% as scale modelers. As is evident by the fact that those three numbers don’t add up to 100%, there is a substantial overlap, with many members enjoying two or more aspects of the hobby. FYI, 227 members identified themselves as AF only, 46 as hi-rail only, and 218 as scale only.

Don’s comment was that “It should be noted, that only one member indicated that he modeled narrow gauge only.” Luckily, things have improved over the past 28 years. We now have seven members who indicate that they are Sn3 only. However, taking other interests into account, 81 members currently indicate an interest in Sn3, 28 in Sn2, and 19 in Sn42.

In Don’s article he includes a graph of the modeling era’s expressed in the survey responses. The 1940-to-1950 time period had 200 members (out of 583 who responded to his survey) and the 1950-to-1960 time period had 221 members, which were the two most popular eras. Unfortunately, I have yet to come up with a meaningful way of programmatically showing that information in our Members Only statistics page. The reason being is that members can write anything in the print or online NASG application form for their era of interest. This makes it hard to analyze the free-form text programmatically. Not impossible, just more time-consuming. It is still on my to-do list, though.

Lastly, Don’s survey included a chart that illustrated what prototype railroads members were most interested in. Things have changed a little in the past 28 years, but not much. The numbers include many of the up-to-six preferences members can indicate on the application form.

	1989	2017
PRR	152	269
NYC	132	180
UP	92	181
ATSF	74	127
SP	72	111
B&O	68	158
C&O	45	59
NP	42	72
N&W	41	69
NH	40	91

Read on to the next page to see what Peter Vanvliet has to say about turntables.

WHEN MODELING

TURNTABLES . . . *By Peter Vanvliet, NASG Webmaster*

A discussion about S-scale turntables on the Yahoo S-scale groups list was the prompt for this section. There are various sizes of turntables and transfer tables available in S-scale. HO-scale tables can be used for Sn3/Sn2 applications with a bit of modeler's effort. Visit the NASG web site's **S Resources** section for links to these companies' web sites:

AAA Precision Turntables produces aluminum-based turntables in all scales, including S. They offer options such as an indexing system, track installation, and bridge detailing, if you prefer to have them do all of that.

Bowser used to produce S-scale turntables, so it might be possible to find them on the secondary market.

Dallee Electronics produces a turntable indexer, available for sale via Port Lines Hobby Supplies. It uses infrared for its alignment positions. Note that this is just the indexer, not a motor or any other parts.

Diamond Scale Products produces a 65-foot turntable kit for Sn3. However, since these are craftsman-style kits, any number of their kits, given a sufficient pit wall height and bridge width, could be used for an S-scale standard-gauge turntable. They sell an indexing and drive system separately. I owned one of their 135-foot N-scale turntable kits. They provide you with all the raw materials and instructions, but they are a challenge to build.

Kitwood Hill Models, a British company, produces various styles of On30 turntables. I've read feedback that Sn3 modelers have successfully used them on their layouts. Some might be useable for smaller standard-gauge turntables as well.

Millhouse River Studio produces high-end turntables and transfer tables primarily in O-scale. They also sell an indexing system and bridge detailing options. They don't specifically mention transfer tables for S-scale, but they mention S-scale turntables on their web site. However, it appears from their web site that they will work with you to get you what you need. Their web site has videos showing their system in action.

New York Railway Supply produces a turntable auto-indexer. Be sure to visit their web site, because apparently the main memory input/output circuit that they used to use is no longer manufactured, and so the system needs to be re-designed to use another component. The current owner of the company is an Sn3 modeler.

Port Lines Hobby Supplies started carrying an S-scale turntable in 2016. Available in sizes from 16" to 32" and various pit wall heights to suit your needs. See their web site homepage and do a text search for turntable to find more info.

If I missed any available turntables that are suitable for S-scale modelers, please let me know at webmaster@nasg.org so that I can keep our web site up-to-date. 🚂

State-Line S Gaugers invite you to join them at the **42nd Annual Fall S Fest** *October 27-29, 2017 • Janesville, Wisconsin*

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Car #1 Side A



Car #1 Side B

All cars are Hi-Rail with AF type couplers with silver tops, red ends and yellow sides

There will be an extremely limited number of cars/sets available — Order Early

If you attended Fall S Fest in the last five years you should receive registration, hotel and car order forms via USPS in April 2017. If you have not attended a Fall S Fest you may obtain material on line or by request from the registrar.

email: behlest@yahoo.com
Or on line at:
www.state-linesgaugers.org

Tom Behles, 2017 S Fest Registrar
15804 O'Brien Road
Harvard, Illinois 60033

T-TRACK S

By Charles Malinowski, Photos by the author

How it all began

Since moving to Carmel, Indiana 18 years ago, I have had a strong desire to take my trains on the road. This started out with the **Adorable** (named by my wife Barbara). The Adorable was an HO oval of snap-track laid on a hollow-core door. The Adorable served me very well until I made the switch to S scale in 2001. One of my best decisions ever!

At the request of our local historical society I built my second portable layout, The Sportable, for an open house at the society's Carmel Monon Depot. The 4 x 6 foot Sportable has made appearances throughout Indiana, Ohio, Illinois, Wisconsin and Michigan, also gracing the cover of the NASG Dispatch! Even though the Sportable was built with a frame of 1" x 2" lumber and 2-inch thick foam base, it became a challenge to transport. After a couple of years I traded in our small pick-up truck on an SUV. So transportation of the Sportable was now dependent on renting a van or my buddy Mick Hinkle's pick-up truck.



Charles Malinowski, left, and Mick Hinkle with S Portable at the Farm Toy Show.

Displaying a layout is great fun. Mick models S in both scale and high-rail. The Sportable was constructed as an S scale layout with hand-laid switches and code 100 flex track. Due to the incompatibility of Hi-Rail/Scale, I felt bad that Mick and I could not fully "Play Trains" together on the Sportable. Then the October 2014 issue of Model Railroad Hobbyist arrived!

One of the featured articles was the MRH Contest 3rd Place Layout. An interesting article to help someone get started in model railroading, it introduced to me to the T-Trak modular layout approach. Referenced in the article was the web site <http://www.t-trak.org> that presented a table listing the T-Trak module standards in a variety of sizes including S scale. The T-Trak standard is SHS/MTH S-Trax. Great! Now I found a system that will allow Mick and I to play fully with OUR layout regardless of Scale/High-Rail/Flyer or AC/DC/DCC.

T-Trak background

Originally conceived for N scale Kato brand track, T-Trak takes advantage of two basic concepts: First, use commercial track components that provide both positive mechanical and electri-

cal connections, and Second, using common folding tables (the "T" in T-Trak) as a support base, thus eliminating legs and the engineering that goes into making that system work. These simplified approaches are huge advantages when trying to recruit people new to model railroading or existing modelers to give S a try. T-Trak-S maybe something NEW but it is something OLD too. Way before my time in S there was a modular system from American Flyer called *All Aboard Scenic Panels*. This was a quick and easy way to build your layout on the floor or on a tabletop.



The Farm Toy Connection

While Mick and I were displaying the Sportable at a 2012 local NMRA meet in Central Indiana, Alan Chesnut approached us about bringing our S layout to his Farm Toy Show. Alan is the host of the Indianapolis Farm Toy Show that had its start over 30 years ago in Lafayette, Indiana.

In 2013 Mick and I attended Alan's Farm Toy Show and we were happily surprised to see so much S Scale stuff at the show! But when we said S Scale, the Farm Toy Folks had no idea of what us train guys were describing. The Farm Folks refer to their models and modeling in ratio terms, 1:64. Alan knew this as he and his sons have both model trains and model farm equipment. Now we were getting somewhere.



Blake Chesnut displaying his farm diorama at the Farm Toy Show.

T-Trak-S layout on folding tables in Charles' basement.



The Sportable was well received at Alan's Farm Toy Shows. We had many curious lookers and wonderful comments but no interest in building a model railroad in 1:64.

While checking-out the Farm Toy Show I took in the diorama room. I was blown away with seeing a dozen or so farm scene dioramas built in S Scale. S track was all that was needed for the farm dioramas to become S MODULES. Also really neat is that construction of the farm dioramas is a FAMILY project. What a wonderful opportunity to grow our hobby and chosen scale.

This is where the October 2014 MRH article comes into play. Based on the MRH article and the standards published on <http://www.t-trak.org> I was determined to create S scale farm dioramas or modules.

It turned out that 2016 was my year for T-Trak-S, debuting at Alan's Farm Toy Show in March with the addition of two modules that Alan and his son Blake built. A great start!

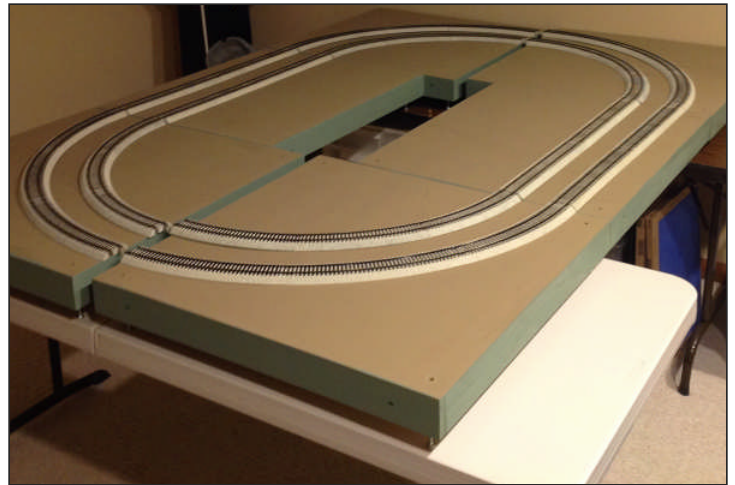
T-Trak-S was also displayed at the Cleveland, Ohio Spring S Spree, the NMRA National Train Show in Indianapolis, Indiana and at the NASG National Convention in Novi, Michigan. These events proved that the T-Trak concepts worked as imagined and the layout operated flawlessly.

As an added benefit, the T-Trak-S layout has helped bring a rebirth to the dormant Hoosier S Gauger's Club. Many of you may recall our Carmel Indiana S Show.



*Standing: Mick Hinkle, Charles Malinowski, Stan Maddox.
Sitting: John Henning and Bob Morrison.
at the NMRA 2016 National Train Show, Indianapolis, Indiana.*

Below: Basic T-Trak-S loops without scenery.



*Straight
T-Trak-S module
ready for transport.*

Basic T-Trak-S Standards

The two basic building blocks for T-Trak, regardless of scale, are corners and straight modules.

- * Corner modules are 28 inches by 28 inches with curve radii of 20 inches and 25 inches.
- * Straight modules utilize 30 inches of track and a module length of 29.875 inches.
- * The slight overhang of track ensures a positive connection between modules. An overhang is also used on the corners.
- * Module heights are a minimum of 2.75 inches to a maximum of 4.0 inches.
- * Space between the tracks is 5 inches, determined by the MTH curve radii.

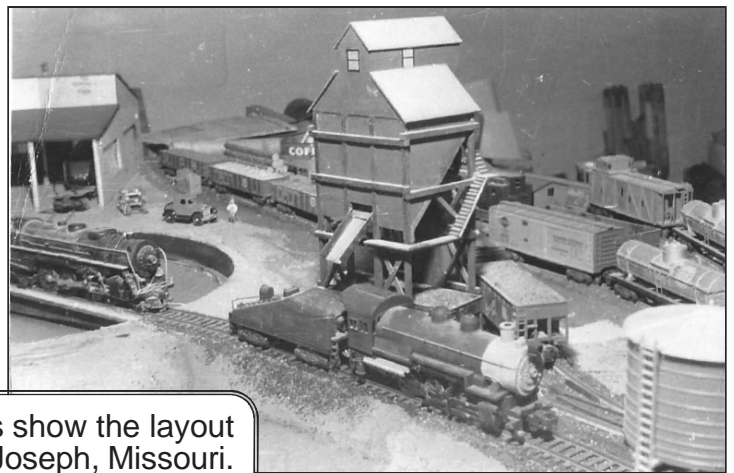
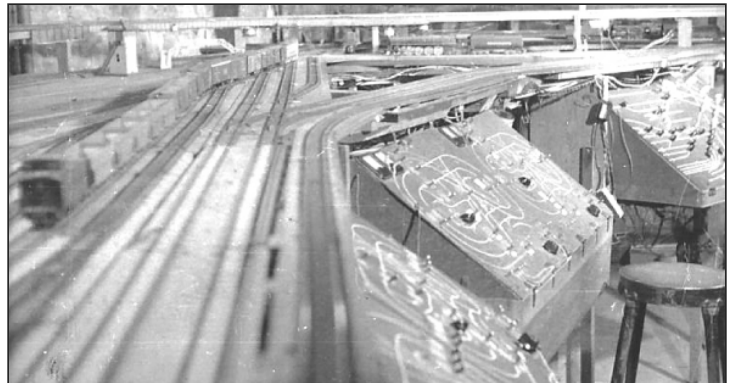
Continued on page 15

55 YEARS AGO IN S

Vic Cherven was given these pictures by **Ron Tilton** who had inherited them from **Jesse Bennett**. Vic sent the pictures to **Bill Winans** with the suggestion that they be published in the Dispatch. Bill passed them on to me, and here they are.

The picture on the right, taken in 1961, is Jim Bresnahan, Mike Porter, Doc Johnson, Dave Engle and Doc's son operating Doc Johnson's layout.

The pictures below, taken in 1962, show Doc Johnson with his layout and another view of the layout.



These four pictures show the layout of Will Estes in St. Joseph, Missouri. They were taken in October 1961.



T-Trak-S Standards *Continued*

* Electrical connection — Each module has separate drops for each set of tracks. I chose to use Fahnestock clips for making my link to power packs. The Fahnestock clips are robust, need no tools for connection after installation, were inexpensive and easily obtained on Amazon.

Lumber for my modules consisted of quality 1/4 inch birch plywood and 1" x 3" dimensional lumber. Inside each module corner I added a 2" x 2" block to attach T-nuts and carriage bolts for leveling.

At home I have three plastic folding tables that are 30" x 72" The extra table surface area provides a plentiful margin of safety if the layout happens to get bumped during display.

Thank you for taking the time to read this article and I hope that I have inspired you to build a module and join in the fun.

Additional resources

Here are some links that will help in your journey into T-Trak. Start with our own NASG. Scroll down to the bottom to find T-Trak information. www.nasg.org/NASG/Standards.php
<http://www.t-trak.org>

<http://model-railroad-hobbyist.com/magazine/mrh-2014-10-oct/500-dollar-layout-contest-3rd-place>

<http://thomas.tuerke.net/on/mrr/?thread=1173489516>

<http://www.lafayettefarmtoyshow.com>

<http://t-trak.nscale.org.au/guidelines>

Model Railroader Magazine, June 2016

*Trackside Photos (T-Trak-N, Project Layout)

*Basic Scenery for a T-Trak Module



EXTRA BOARD

S RELATED ARTICLES IN OTHER PUBLICATIONS

TANKS FOR THE MEMORIES! is only four pages long, but it has seven great illustrations, including five 1946 American Flyer tank cars. Written and photographed by NASG member **Ted Hamler**, it is in the **March 2017** issue of **Classic Toy Trains**.

Also in the **March 2017 Classic Toy Trains** is an article titled **BIG STEEL AND THE PENNSY** by **Roger Carp**. Photographed by **Steve Tressler**, it is the story of **Bill Loop's** unusual S-gauge and O-gauge combination layout.

The **March 2017** issue of **Railroad Model Craftsman** features a picture from **Mike Fyten's** S-scale **Kaw Valley Railroad** and an article and more pictures on pages 42 through 47.

Congratulations to **Dick Karnes** for winning the **RMC/Dremel Kit-bashing Award**. Dick's article on pages 90 through 92 of the **March 2017** issue covers kit-bashed self-powered smooth side and heavyweight commuter and trailer cars using electric power from his catenary with American Models GG-1 pantographs.

END & REBIRTH of S gauge Santa Fe trains in the **May 2017** issue of **Classic Toy Trains** relates the history of the Santa Fe streamliners offered by Gilbert and Lionel over the years. The four pages with five illustrations was written by **George Hoffer**.

STEVE MONSON

Has made a monetary donation to NASG

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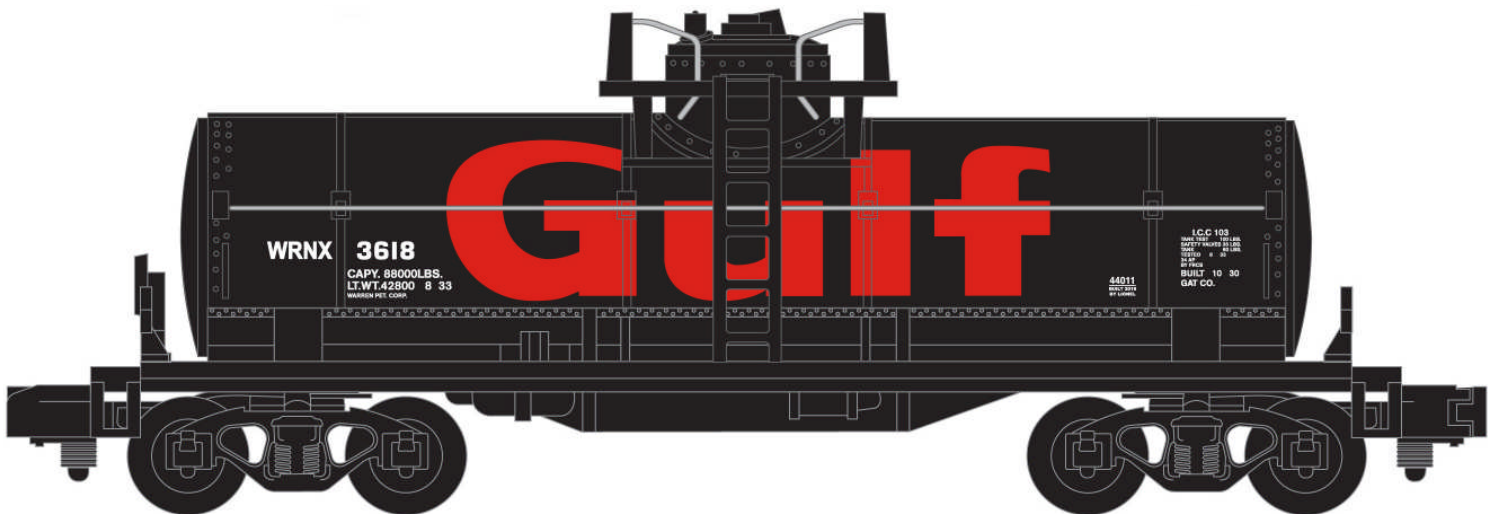
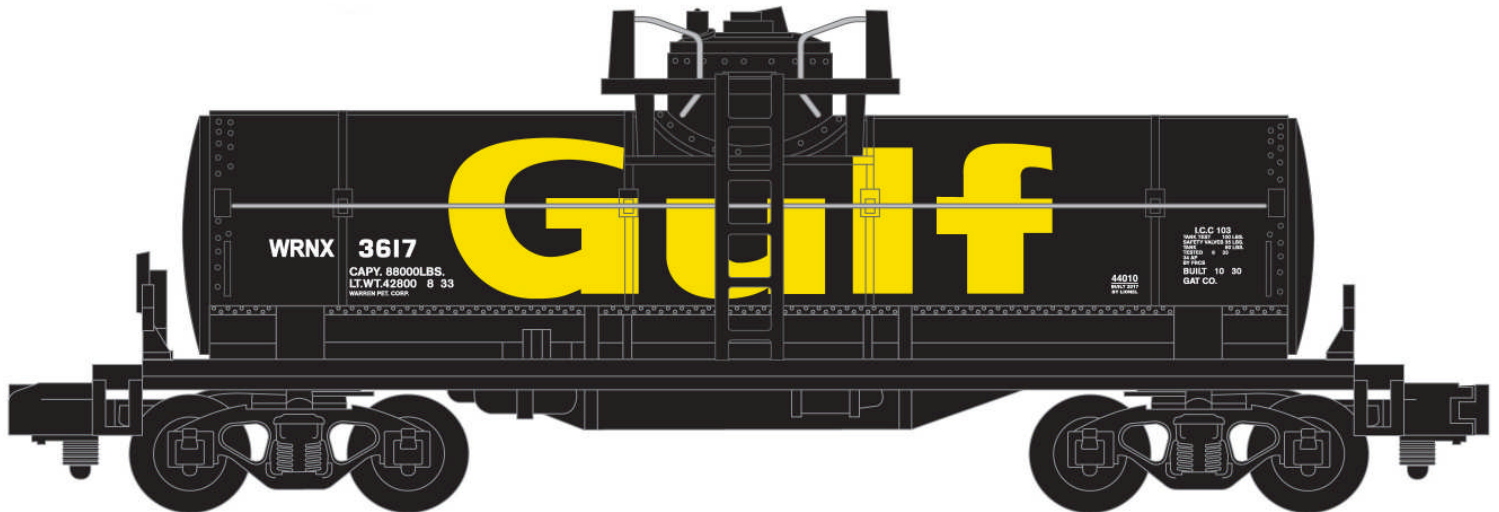
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2017-2018 AF COMMEMORATIVE CARS

Each year since 1981 the NASG contracts with Lionel/American Flyer to produce a limited run locomotive or car for the collector or American Flyer operator. We located photographs of Gulf tank cars with two different color schemes and decided to use them as the basis for both the 2017 and 2018 commemorative cars. They will be produced in the same production run and both will be available this year.



The price of these cars is \$69 each or \$130 for two

This is an official NASG sponsored project so you must be an active member to purchase a car. **TO ORDER THESE CARS** make payment out to NASG and mail to: Dave Blum, 3315 Timberfield Lane, Pikesville, MD 21208

Please include the following in your correspondence:

1. Your NASG ID number from your membership card.
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GEAR REPLACEMENT S HELPER 2-8-0

By Steve Monson,
Brooklyn Park, Minnesota
Photos by the author

The S Helper 2-8-0 is a very fine steam locomotive. Its smooth quiet operation is unsurpassed by any other locomotive of its size, and it has great detail. Unfortunately the drive gear has failed in a number of these locomotives. It simply is too small for the job. Fortunately with a little care the gear can be replaced without much difficulty. It remains to be seen if the new gear is better than the old. It is the same size, but hopefully made of a better *Delrin* material.

Replacing this gear is not difficult. You probably have all the tools that you need. However, there are some fragile details, so be careful and don't hurry.

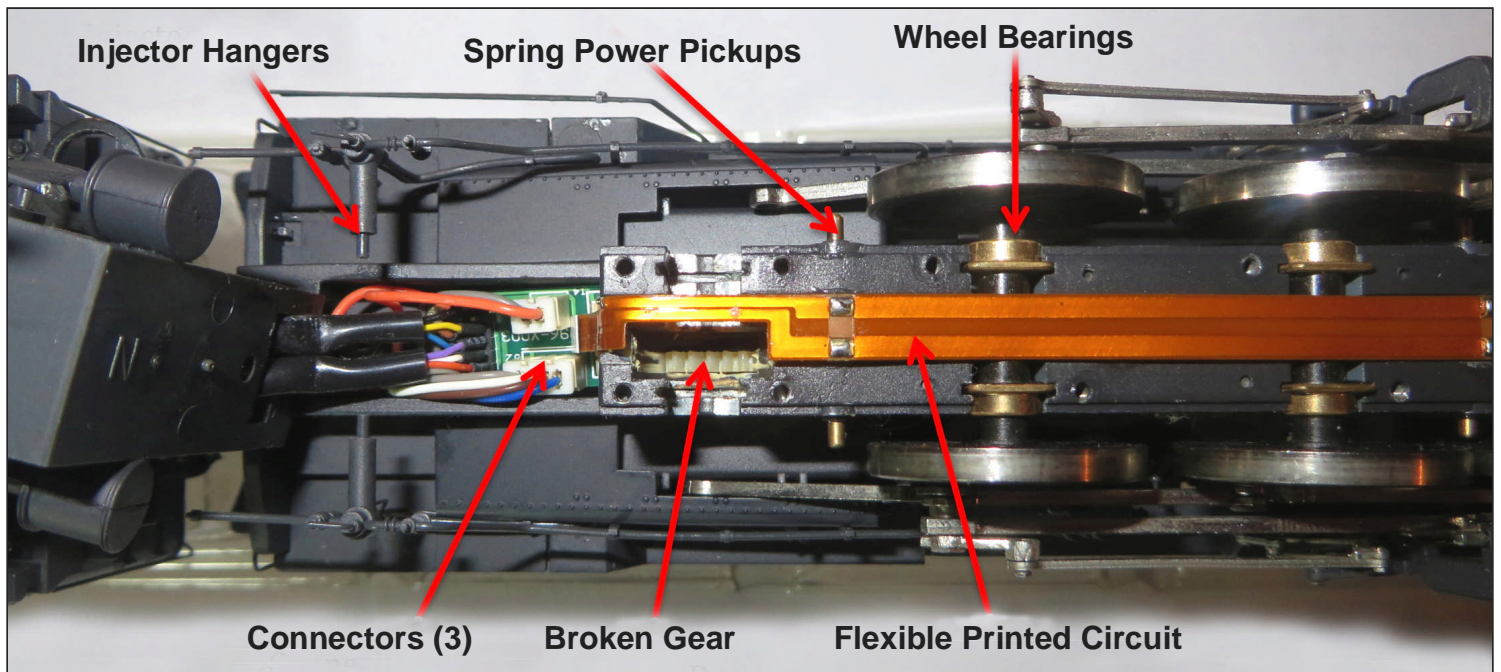


Preparation

The necessary tools are pretty basic and are listed at the end of this article. The vise makes the job of re-quartering the drivers easy. Mine came out perfect the first time. I suggest working in a brightly lit area and have a jar or envelope ready to store the 20 very small parts that you will remove. Place the locomotive and tender on their back in the original clear plastic packing material. If you no longer have it, make a Styrofoam or foam rubber cradle for the cab roof and place another piece near the front of the boiler. You do not want the locomotive to rest on its bell. The tender will not be removed so place something under it as well.

Rear Driver Removal

1. Remove the side rod screws from the rear pair of drivers. A hex wrench is nice, but a small pair of pliers will work fine. Be careful — under each side rod is a thin washer. It may stick to the driver. Also, inside each side rod is a small cylindrical bushing. Place all six pieces in your safe keeping container.
2. Remove the front pilot truck. There is one screw and no washers.
3. Remove the 11 bottom plate/brake hanger screws plus one long one at the very rear under the cab. Remove the tender drawbar screw with its sleeve. Your safe keeping container should now have 20 pieces, plus a front pilot truck.



Tip the bottom plate up and to the left as shown in the photo above. The photo shows the important points we will work with. There are two 1/8" x 3/8" black cylinders extending out from

the rear part of the frame that hold the injectors. They are not glued and they can be pulled out horizontally.

Continued on the next page

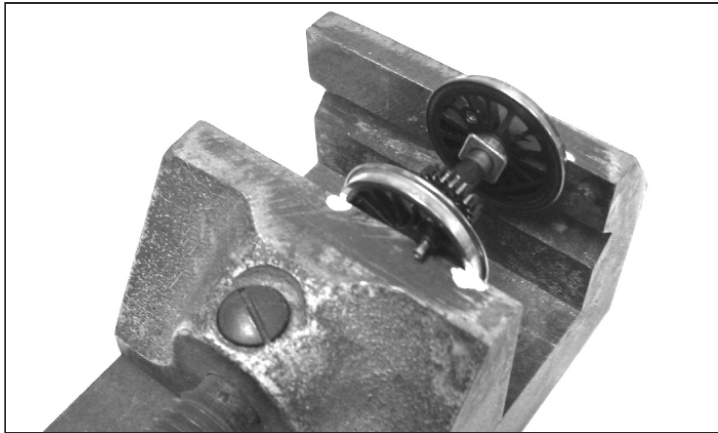
GEAR REPLACEMENT S HELPER 2-8-0

You will see some connectors on the rear end of the frame. These may vary depending on whether you have AC, DC, DCC or sound. Under the bottom plate is a flexible printed circuit. This provides power to the lights and smoke and is connected to the driver pickup contacts and the chuff synchronization. Its connector is extremely fragile. Never under any circumstances pull on it by the flexible printed circuit wires.

The junction block with all three connectors is not fastened down. You may be able to lift it up far enough so that the rear set of drivers can be removed without unplugging the printed circuit connector. Or you could remove the two connectors on the left side which are much more robust. If you do end up disconnecting it, be very careful.

Riding on the front of each rear driver are electrical pickups, one on each side. On mine they were not doing much good as they no longer pressed against the inside of the drivers. My locomotive was still running well, so this means that most of the power was coming through the tender, which has 8-wheel pickup. Nevertheless, be careful not to damage or lose these pickup posts. Now carefully lift out the rear axle/drivers.

If the synchronization circuit gets broken and you are using DCC, loading 00 into CV112 would give you auto chuff.



Driver Quartering

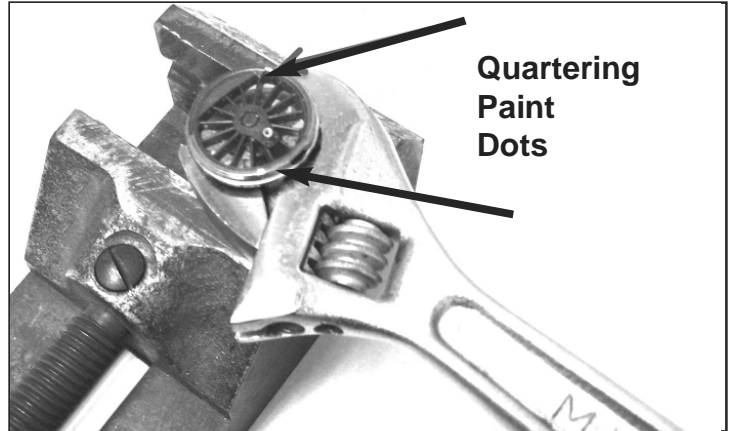
The side rods on the right side of the locomotive lead the left side by 90 degrees. It is very critical that the driver be replaced precisely at 90 degrees to the other. An easy way is to place the wheel set in the vise as shown. Make sure it is in square and level. Place it so it sits on the black flanges around each main rod bearing. Then paint a small mark on the rims of each driver just above the edges of the vise. Paint them carefully and let them dry before removing the wheel set.

Driver Removal

Remove the driver that is closest to the gear. The gear will come off only from one end. I used a crescent wrench laid on top of the vise to hold it as shown and tapped out the axle with a small punch. The wheel will come off easily.

Re-assembly

Dab a tiny amount of epoxy on the splines where the gear will seat. Press the gear onto the shaft all the way to the stop using your vise and a small tube to push with. A 5/16" socket works fine. Place the drivers and axle in the vise just the way you had them when you painted the rims. Again I used a touch



of epoxy. When satisfied press the driver onto the shaft. Align the bearings properly and place the drivers back in the frame. Mate any connectors you took apart. Install all screws and insert the injector mounts. My locomotive now runs as smoothly as it did before the gear broke. I plan on running it just as hard as before and see if the new gear is tougher than the original.

Purchasing the gear

Northwest Short Line part number 2930-6

Go to: www.NWSL.com

Click on: Store

Quick Search: 2930-6

Tools: Vice, Small screwdrivers, Tweezers, Small crescent wrench, Small hammer, Epoxy glue.

Extra Loco Upgrade

The tender on this locomotive is very lightweight. There are two bundles of wires that run from the tender to the locomotive. For all but the very broadest of curves, these tend to pull the front of the tender to the side and cause the front truck to derail. While you have the locomotive on your workbench, it is easy to add some extra weight.

The tender shell is fastened with just two screws in the front, and is hinged in the back. Be careful not to break the two plastic hinges. There is room for a 1½" x 1½" x ¼" weight under the coal bunker. I used three large washers from a hardware store, and glued them in with Household Goop. This gave enough weight to eliminate the derailments I had been having. I let it dry several hours, as it tends to move around after it appears to be dry. Be careful of the rear coupler lift bar when replacing the tender shell. Install the two screws and you are done.

With a new gear and some extra weight in the tender my locomotive is running better than ever. 🚂



KEEP THEM DOORS OPEN

By Edwin C. Kirstatter
Northern Ohio S Scalers

Keeping Roundhouse doors open was accomplish in many ways. This is how the Baltimore and Ohio did it in Cleveland, Ohio down in the 'Flats' of the Cuyahoga River at the West Third Street Roundhouse. These devices kept the doors open while Locomotives passed by. It was done this way at other engine houses. It must have been a standard design. Dimensions were taken from these to make the plans to build accurate S scale models.



Ed Kirstatter doing "crouch down" research, going right to the source to get it right, correct and accurate.

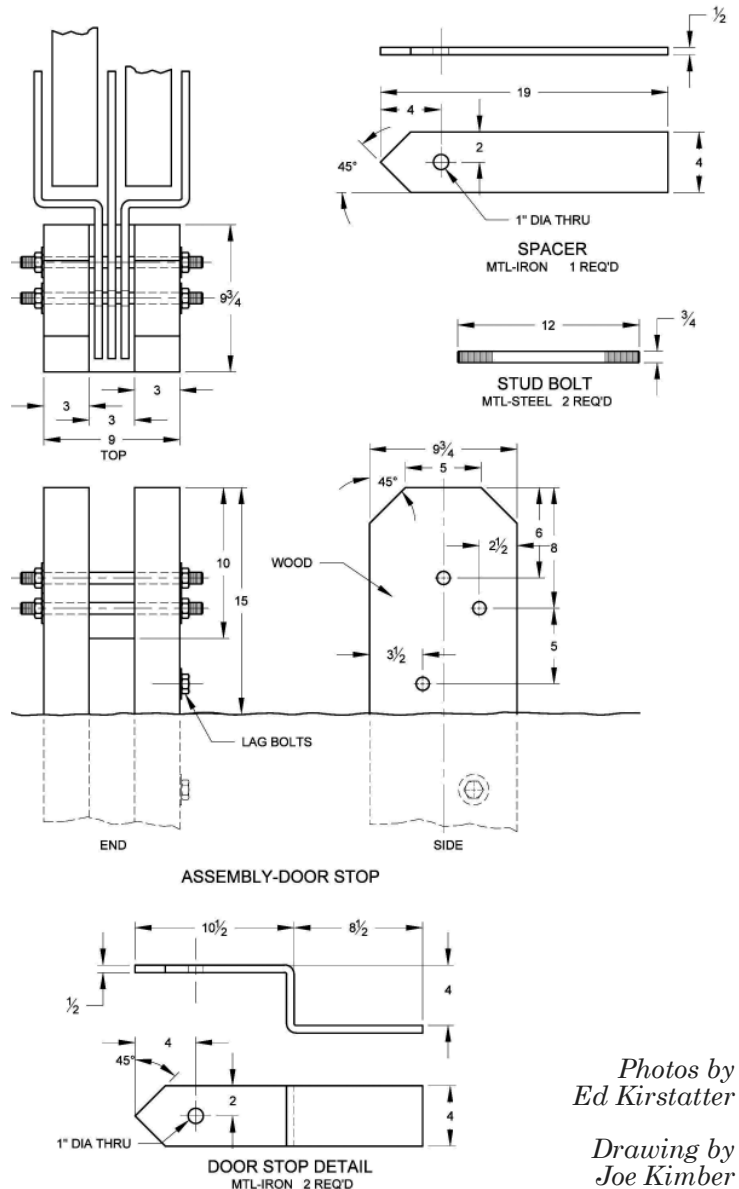
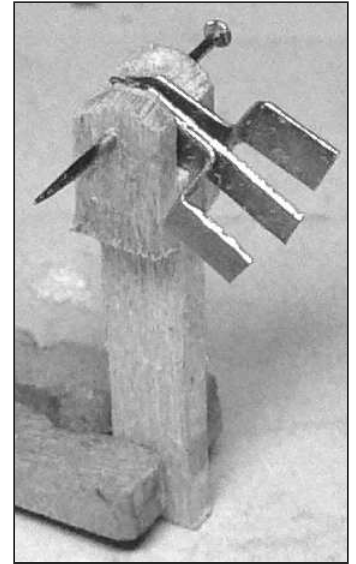


Door-stop left behind at the site of the Ohio Roundhouse.

Parts for these are made from wood or plastics, brass and pins and many other parts. The real ones had creosote coated timbers and 1/2" thick Iron door stops with 3/4" hex bolts, nuts and washers. There may have been track ties sunken three feet deep in the ground. We will just cut ours off at the ground level. Work from my sketches and photographs to build yours. You may want to build it a little oversize due to its small size.

Start by cutting two timbers 3"x 9 3/4" (1/16"x 3/16") to the height of 15" or 16" (1/4") above ground with a spacer between them of the same size as Northeastern's wood, left long to use as a handle while cementing and assembling the parts and sanding the tops at 45-degree angles to clear the Zee bars so that they can swing over the top. Some have been seen that were rounded on top. Drill holes for the cross bolts made from .020" wire or Bank pins and make it a tight fit. A second bolt is required to keep the irons up in place beside the doors and rest on it. A third square headed Lag bolt that is down lower is to hold it all together. We could use Tichy Train Group's #8142 Nut-Bolt-Washer plastic moldings to simulate the ends of these bolts. Kadee has some of these cast from metal. File a pin head square for the lag bolt. Cut the door stop irons from K&S #251 brass .010" sheet or use Tinplate formed in a Zee shape with holes drilled oversized for the bolts to pass through them easily. There is a door spacer iron between these.

Blacken the brass parts and make the posts look like well weathered creosote soaked wood. To assemble these parts put the door stops (Irons) between them with the top bolt through those and resting on a bolt below with the N-B-W castings in place at the outer ends. Now cut off your handle piece below ground level or round it off to be pressed into a hole drilled in the ground and glue these on the layout in front of the roundhouse doors to make it look like they are buried in that place. You can then cover this area with ashes, dirt, grass, weeds, etc.



Photos by
Ed Kirstatter

Drawing by
Joe Kimber

PRODUCT REVIEWS

STEAM-POWERED PILE DRIVER

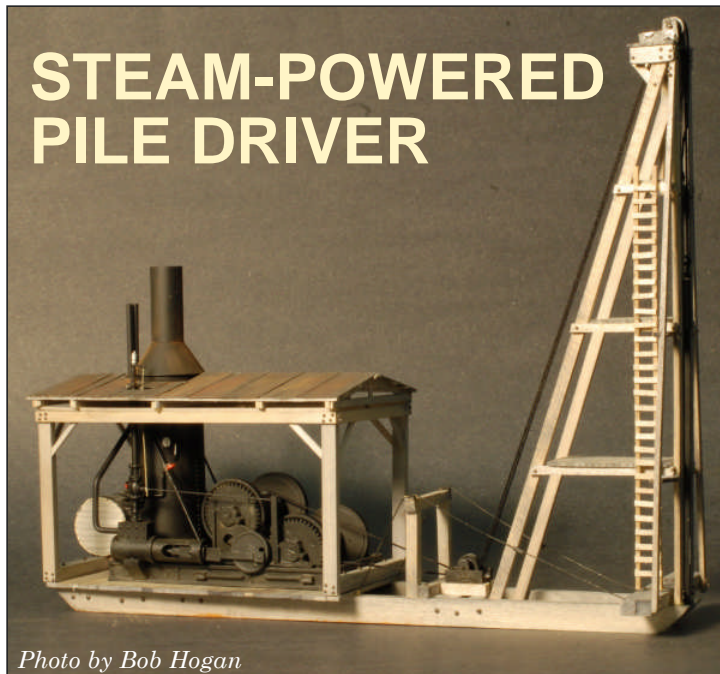


Photo by Bob Hogan

KMP Models of Corvallis, Oregon has added an S scale version of its steam-powered pile driver that has been previously available in O, HO and N scales. The assembled model has a footprint of 9³/₄" long by 2⁵/₈" wide and is 8¹/₄" high and it would be a unique addition to any S or Sn3 layout.

The kit has 52 white metal castings that are of excellent quality. There are 45 pieces of high quality basswood in 14 different sizes, along with a cast resin boiler, brass smoke stack, plenty of Grandt Line styrene NBW's in two sizes, corrugated metal roofing and other plastic parts. The kit includes five pages of excellent dimensional drawings, a 10 page instruction packet plus six pages of color photos showing the various stages of construction and the completed model.

This kit is easy to assemble and will give the builder lots of satisfaction when completed. The pile driver kit has two major components; the steam engine/winch assembly and the wood pile driver platform. The detailed instructions, full sized drawings and color photos make the platform construction very easy. I recommend that one should distress and stain the basswood parts before beginning construction of any kit. These basswood components go together using white glue and any good CA will work to assemble the various white metal parts on the steam engine/winch assembly. The completed model adds a lot of interest to any model railroad scene

There is more to look forward to from KMP Models for S scale logging modelers. Next up in S scale is a train of five logging camp cars including a kitchen car, three different bunk cars and a supply car.

Bob Hogan

The list price for this kit is \$198.00

KMP Models are available at many hobby shops nationwide. See the list on their website www.Kmpcraftsmankits.com or you can e-mail them at kmpmodels@yahoo.com

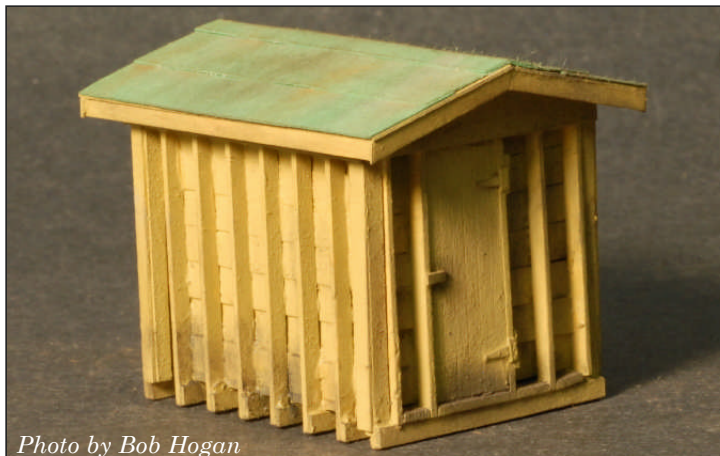


Photo by Bob Hogan

D&RGW CUMBRES COAL SHED

In 2016 **Dan Peterson** of **San Juan Decals** began expanding his product line to include narrow gauge structure kits. His first kit, the Cumbres section house coal shed, initially offered in O Scale, and it is now also available in S scale.

The prototype is located adjacent to the D&RGW section house on Cumbres Pass and still exists today, although it is in poor condition. The prototype shed is an 8' X 10' wood structure with exterior framing designed to store coal for use in the section house stoves. A similar shed still exists in the Chama yard. This type of shed, without a foundation, was probably built off-site and placed at different locations as needed.

This is the most intricate small structure kit that I have ever built. It is a laser-cut kit, but one that utilizes several construction techniques that I have never seen before in any type of kit. This is not a "shake the box" kit or one that you can throw together in an evening or two. The finished structure is worth the time you spend building it. There are three pages of detailed instructions with step-by-step computer generated color drawings to guide you through the construction process. The instructions include construction drawings and color photos of both the prototype and completed model.

The San Juan kit was designed to be built similar to the way that the prototype shed was by using pre-sized/cut basswood 2" X 6" wood strips for floor joists, wall studs, stud caps, etc. Three laser-cut cardboard jigs help the builder construct the side stud and floor joist U-shaped assemblies and then serve to hold these assemblies together in a vertical position so the builder can more easily add the 1" X 12" interior wall planking. The shed floor, door, roof end/side pieces, roof support/rafters and the sub-roof are all typical laser-cut ply pieces. San Juan Decals includes Grandt Line styrene castings for the door hinges. The choice of roofing material (tar paper, shingles, corrugated steel, etc.) is left to the builder as these structures were used in various ways and styles all over the D&RGW and C&S narrow gauge systems.

Bob Hogan

The list price for this kit is \$22.95

These kits are available at local hobby stores or can be ordered directly at www.sanjuandecals.com

NEW WEST JERSEY RAILROAD

Part 2



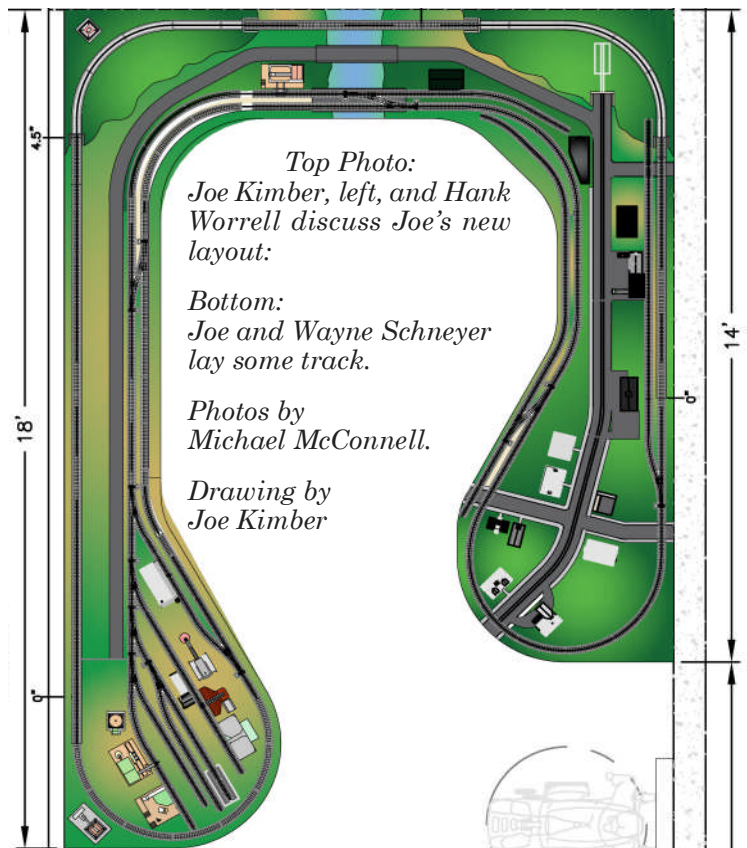
By Joe Kimber, South Jersey S Gaugers

Work continues on the “New” West Jersey Railroad. The tables are complete, track bed is glued down and track is being laid. Wayne Schneyer is doing much of the work for me. Wayne is a member of the South Jersey S Gaugers and volunteered to help last year when I was still in the planning stage. He has been coming down one or two days a week and working 4-6 hours. I am capable of less and less as my illness progresses. If not for him, I would not have a layout.

I decided to reuse the American Models track from my old layout. Most of what was salvaged is still in good condition. At this time we have approximately 100% of the track down and spiked. The layout is divided into three blocks. Primarily for trouble shooting and signals. There will be a set of 14-gauge bus wires for each block. The DCC power to the track is fed from the buss wires at about 6' intervals. Now that the track is complete, we will start the buss wiring. We will need 4 sets of buss wires for track power and one set each for AC and DC power. The AC buss wires are 14-gauge black/white. The DC buss wires are 14-gauge red/black. These colors are industry standard. The AC power will be for powering the accessories and some lighting and the DC power will be for some signal lighting (LEDs) and powering the stall type turnout motors.

I have been contemplating wireless control for my DCC system but have not made a decision yet. If I choose wireless I can eliminate the cable and panels for the hand held cabs. However, the layout is probably not large enough to justify wireless control. I purchased an NCE Power Pro system for my old layout and expanded it with an SB5 Booster. I will probably purchase several cab panels and continue to use it as is.

By the time you read this, wiring should be well under way. I plan to operate the layout for a while to work out any track bugs and operational issues before starting the scenery. I will fabricate the girder bridge and trestle next. The bridges will be installed before other scenery is in place. **to be continued**



THE CONDUCTOR'S CLIPBOARD



By Bill Pyper, Editor, NASG Dispatch PROMOTING S

On January 28th and 29th, 2017, the hard-working Membership Secretary of the NASG, **Dick Kramer**, attended the **Railroad Hobby Show** presented by the **Amherst Railway Society** in West Springfield, Massachusetts. This is an annual two-day show, held in four enormous buildings and has an attendance of 25,000! Through the generosity of the **Bristol S Gaugers**, the NASG had

a table next to their layout. Dick created a presentation which promoted the NASG and he handed out copies of The Dispatch along with manufacturers brochures. Special thanks to Ron Bashista of American Models for getting 25 copies of their brochure to Dick in time for the show. 🚂



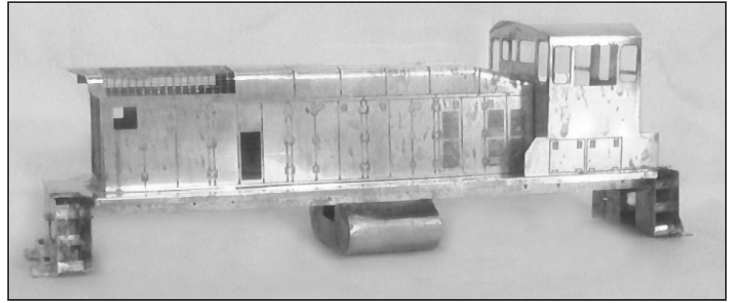
Above: Dick Kramer at the Railroad Hobby show.

Club changes name and looks for new members

Due to the multi-scale diversity in the club, it was decided by a vote of club members to change their name from *Pioneer Valley S-Gaugers* to **Pioneer Valley Model Railroad Club**.

Also, the **Pioneer Valley Model Railroad Club** is seeking to enroll new members from northern Connecticut, western Massachusetts, southwestern New Hampshire, and southeastern Vermont. They are a multi-scale model railroad club, with strong ties to S-Gauge. They are seeking interested members from all scales to join their diverse group. Since 2009 they have been involved with the Look Memorial Park in Florence, Massachusetts and their holiday model train exhibit known as **Santa's Trains at Look Park**. If you are interested please send an e-mail to Steven Allen at allens1017@comcast.net for further information. 🚂

UPDATES . . .



March 3, 2017 — The **Gilmaur S scale SW1500** etched brass body and chassis kits are ready for production. The price is \$160 plus shipping. If interested, please contact Colin Stewart at soo4513@btinternet.com

Sam Powell's Layout

By Roger Schneider, Pittsburgh S Gaugers

To bring you up to date on the Sam Powell layout project, looking toward an eventual Article Part II. Progress over the winter months has been very slow. Individual schedules have been full with other things to do, PLUS the space where the layout is located only got heat about three weeks ago!! We've just now started back doing a little work one day a week. We have all of the layout assembled now and in place. The next step, which is going a bit slower, is doing some serious track-work where we had to make some modifications to the bench-work to fit in the new room. Also we are still working on hooking up all the wiring for power to the tracks and for the Lenz DCC system Sam used. We have some areas of scenery to repair and also some to build. Some of the background mountain areas had never been fully completed on the original layout and we're reinventing those to match what Sam had done. Just last week I was finally able to run one of Sam's engines about 20 feet along a section of track that was intact using the DCC system. Sam left no wiring diagrams, so I'm still trying to figure out a few sections of his wiring. I got his wonderful turntable to work last week, so that's a major item off my checklist!

We have a self-imposed deadline of having the layout in presentable form and able to run a train or two on the gigantic outer loop around the entire layout for Monday, June 26. The local region of the TCA is hosting their 2017 national convention June 25 to July 1 in Pittsburgh and Sam's layout will be on the area layout tours. I suspect that once that week is over, I'll be able to get you photos and information for the second part of the article. You already have the track plan, although we're going to have to update it slightly once we complete the re-laying of tracks in a roughly 10" section of the layout we had to remove to provide more room for visitors to enter the layout! This area is in a yard plus main line so we have about 10 tracks to re-do! We had to take a slice out of the entire width of the layout in that section. 🚂

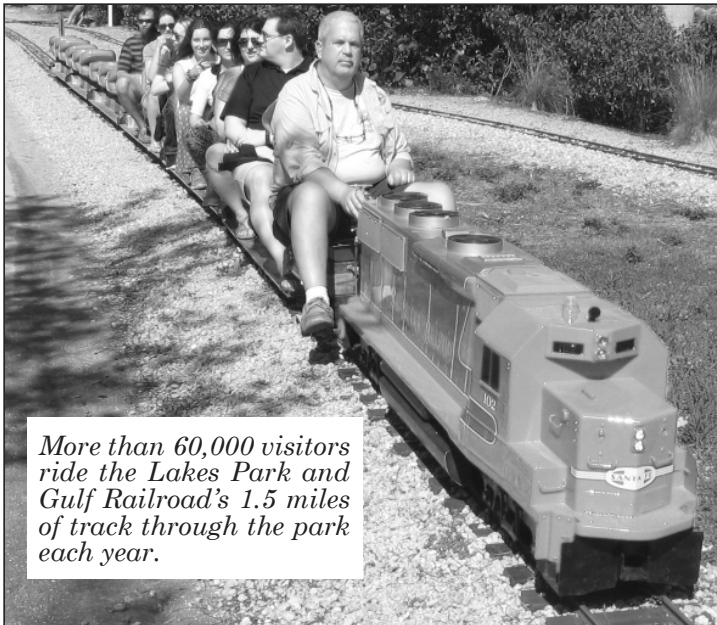
Editor's Note: Dawson, Pennsylvania (near Pittsburgh) is not far out-of-the-way for many of you who will be driving to the NASG Convention in Baltimore from Illinois, Indiana, Michigan, Ohio, Wisconsin and parts west. Maybe you can stop in and see Sam's layout in its new home. 🚂

RAILROAD MUSEUM of S. FLORIDA

A Hidden Treasure in Southwest Florida By Marty Lydecker Photos by the author

After years of public service, *retirement* is now a reality. I decided the time had arrived that I could begin working on the many hobby projects I had put off for so long. As a collector of American Flyer for the past 35 years, my layout, displays, and an inventory were at the top of the list. But as circumstances would have it, my northern Ohio *man cave* now had to begin a *time share* with a condo in southwest Florida.

I had some HO trains and Aurora slot cars as a youngster. They made the trip south and I set up a small layout on the lanai. It was fun playing with the grandkids for a week, but my layout just did not fit in with the palm tree patio seat cushions, so I went searching for a better remedy for my train habit.

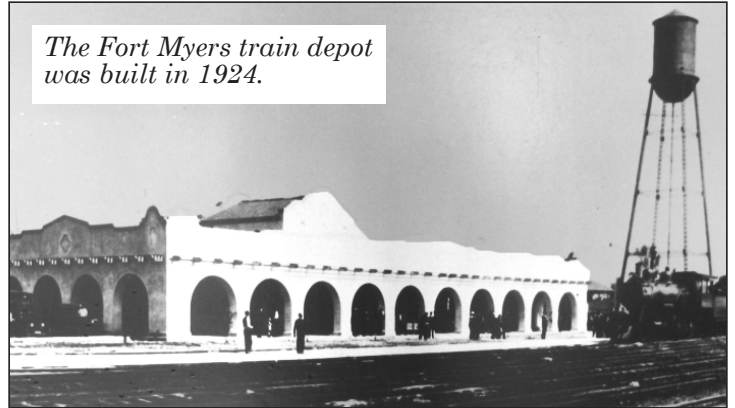


More than 60,000 visitors ride the Lakes Park and Gulf Railroad's 1.5 miles of track through the park each year.

In Ft. Myers Florida our family often gathers at Lakes Park. We enjoy the biking, hiking, and playgrounds. But our favorite destination is the train ride which travels through the park. It is the 1/8 scale **Lakes Park and Gulf Railroad**, a 1½ mile, 15 minute excursion with tunnels, bridges, structures, natural vegetation, and wildlife. The ride is often highlighted by alligators taking a swim or sunning on an island. Lakes Park is home to the **Railroad Museum of South Florida**, which runs the train and uses the proceeds to help fund the museum. Inside the museum, there is a movie room, a library, a nice collection of railroad history, an overhead G Scale train, a new GE Engine simulator, a Lionel display, and an N Scale layout with two mainlines and DCC control. What really makes the museum a special place for me, is the 9½ x 20 foot S Scale layout!

There was much discussion by the RRMSF about what gauge to model, O, HO, or S. In 2012, a decision to build an S SCALE layout was made. It took over two years of planning, contracting, and building to come up with a layout that is absolutely first rate.

The theme of the layout is late 1950's/early 60's railroading in Florida. The most impressive structure is a 30" x 9" scale replica of the Fort Myers Train Depot. Built in 1924, passengers riding the Atlantic Coast Line, and later the Seaboard



The Fort Myers train depot was built in 1924.

Coast Line Railroad, would travel from points north to vacation on sunny Fort Myers Beach. Service at the station ended in 1971, however, the building still exists and became the home of the South West Florida Museum of History.

Continued on the next page

Clinics at the 2017 Convention

Continued from page 7

Intro to DCC — Friday 6-7 PM Chesapeake 1

**From the Ground Up by Bob Geldmacher
Friday 7-8 PM Chesapeake 2**

Bob will begin this clinic with a demonstration and review of rock casting using Hydrocal plaster. This will be followed with the discussion and construction of basic landforms using readily available extruded insulation foam and plaster cloth and the placement and coloring of rock castings on that land form. To complete the scene and add great interest for the viewer, Bob will review many of Scenic combination to add interest with color and texture variations to any layout.

**How to Repair American Flyer Diesel Locos
by Tom Barker**

Friday 8-9 PM Chesapeake 1

Tom Barker will provide detailed instructions on how to repair your AF diesels and get them running again. While diesels are trickier to repair, Tom will show you how to do the job smoothly and see those diesels running on your layout once again.

**Trees, Trees, Trees by Bob Geldmacher
Friday 9-10 PM Chesapeake 2**

Bob will demonstrate the use of Scenic Express Super Trees and various ground foams and Super Leaf products to make life like deciduous trees including the highly detailed Sage Brush trees. To add a slight twist to the clinic, Bob will cover and show the step-by-step procedures for making Sisal or Bottle Brush pine trees. The clinic will conclude with the construction of Tall Pines trees that will add that WOW factor to your layout. Bob will be available before and after each clinic to answer any scenery questions convention attendees may have or demonstrate any additional scenery techniques or products of interest.

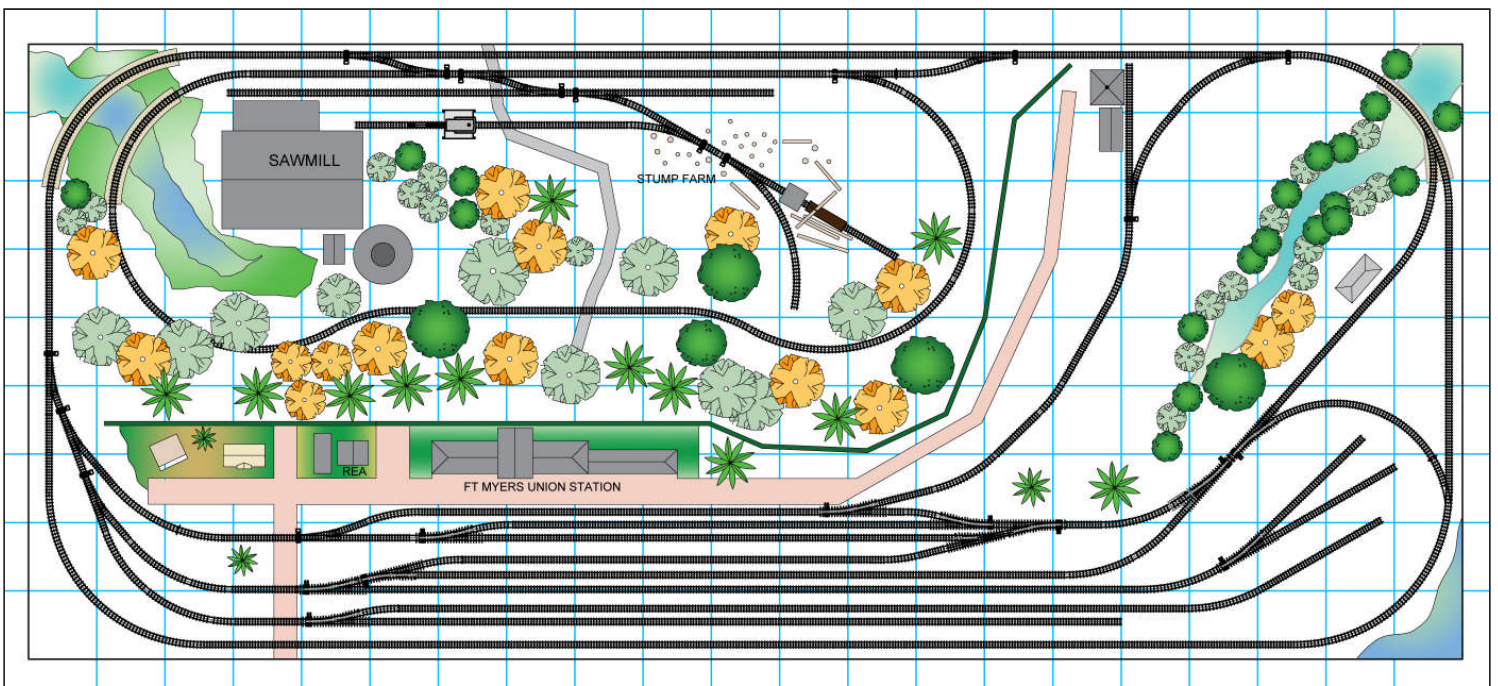
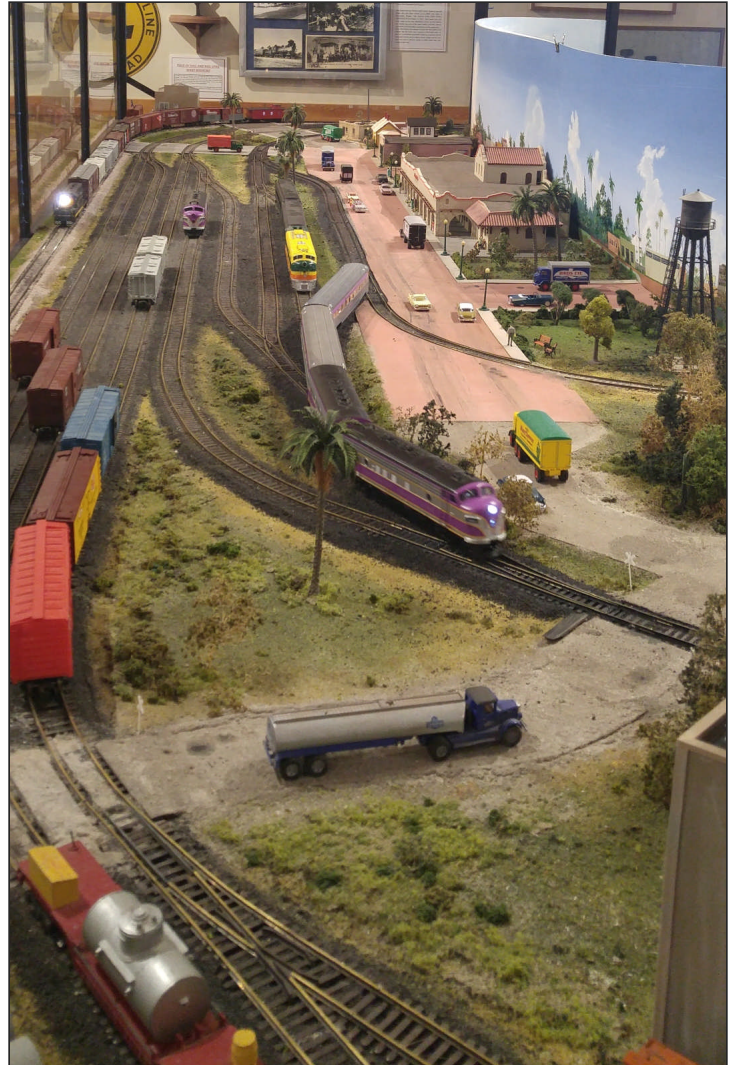
More clinics on page 30

RAILROAD MUSEUM of S. FLORIDA



The outer loop of the layout is shared by a DCC Controlled (NCE) American Models FP7 ACL Passenger Train, with a baggage car and two coaches, and an ACL FP7 Freight Train. I vividly remember those striking Purple and Silver FP7 ACL engines when I traveled by rail from New York to Fort Lauderdale in the early 1960s. As the ACL passenger train passes the station, it is as spectacular a model railroading scene as I have ever seen. In front of the station, an American Models Seaboard Coast Line GP9, with lights and railsounds, sits on a siding. The ACL freight train consists of a new Aztec track cleaning car, a few hoppers and a caboose. Various S Scale and American Flyer freight cars decorate the train yard.

On the backside of the layout is a logging loop. At the turn of the twentieth century, the logging industry was at the center of economic development in Florida, and subsequently, railroading expanded and developed. A DCC controlled American Models Southern Baldwin Switcher works tirelessly, pulling a few flatcars, hoppers, and a caboose. A highly detailed sawmill is featured in the middle of the loop. On the sidings around the layout, you can find American Flyer cranes, reefers, gondolas, flatcars, and cabooses. I collect and model only American Flyer, but on this S Scale Layout, I will admit they are not as realistic as the scale trains.



Drawing by Joe Kimber



There are a few interactive buttons on the layout: an American Flyer 970 Seaboard Walking Brakeman Car, a warehouse with an HO towmotor that runs in circles, and an Ice house with a conveyor belt. The museum will be adding a few more accessory buttons for young visitors to the museum.

After nearly eight months of flawless operation, some unique problems surfaced. The code .148 brass rail tarnished, got cleaned, ran for a few days, and then the cycle repeated. The Florida humidity had taken its toll. So a decision was made to replace all of the brass track with MTH S Helper Nickle plated code .131 track. The engines that were running 4-6 hours a day are now on a six-minute timer, are running at a slower speed, and do not seem to be overheating. Switches with plastic frogs have been converted to handmade nickle silver using a Fast Track mold. There is a concern with brass wheels wearing down. I would appreciate hearing from any S Scalers who have had experience with large layout operations. I have had a great experience volunteering at the RRMSF.

I encourage you to visit and see this incredible layout, and enjoy the museum. The Railroad Museum of South Florida is located at 7330 Gladiolus Drive, Fort Myers, FL 33908. It is open seven days a week. Telephone 239-267-1905. E-mail railroadmuseumofsouthflorida@gmail.com Home of the 1/8 Scale Lake Parks and Gulf Railroad. Also displays the 145 steam locomotive and a Seaboard Caboose.



Southern Railways Baldwin switcher passes the saw mill.



ACL freight train passes the ice house.



AF 970 Seaboard Railroad operating boxcar.



S scale layout in the Railroad Museum of South Florida.

To see **more pictures** from the Railroad Museum of South Florida, visit the NASG web site www.nasg.org and click on **Members Only, Log In** with your member number, click on **Recent Dispatch Issues**, choose the May-June 2017 issue from the menu and scroll to page 34.

CALENDAR OF EVENTS

This page is for the listing of model railroad events, with emphasis given to S-gauge or S-scale. If you know of an upcoming event related to S-scale model railroading, or model railroading in general, please e-mail the information to dispatch@nasg.org as soon as possible so that we are able to print it in a timely manner.

May 4 through 6, 2017 — Marion, Ohio

The **SPRING S SPREE** presented by the **Central Ohio S Gaugers** in the Veterans' Memorial Coliseum at the Marion County Fairgrounds, 220 E. Fairgrounds Street. Free Parking. Over 135 Dealer Tables, How-to Clinics, Operating Layouts, Grand Door Prize, Pizza party Friday night, Pancake breakfast Saturday morning. Visit our web site www.SSpree.Info for complete information and registration form.

May 6, 2017 — Marceline, Missouri

MODEL TRAIN SHOW at the Walsworth Community Center, 124 East Richie. Admission FREE. 9 am to 4 pm. Operating layouts, vendors. BNSF equipment display. Info at www.walsworthcommunitycenter.com

May 6 and 7, 2017 — Painesville, Ohio

THE PAINESVILLE RAILROAD MUSEUM is hosting a **MODEL TRAIN DAYS** with operating layouts in all gauges including S. Open 10 am to 4 pm at the Painesville Depot, 475 Railroad Street. www.painesvillerrailroadmuseum.org

May 7, 2017 — Wheaton, Illinois

CHICAGOLAND ASSOCIATION OF S GAUGERS will have their layout set up at the **Great Midwest Train Show** at the DuPage County Fairgrounds, 2015 Manchester Road.

May 19, 20, and 21, 2017 — San Francisco area, California

FLYER FEST WEST will be held at the Poplar Event Center, 1700 Coyote Point Drive in San Mateo, California. Friday, we'll take a trip on BART for a Cable Car ride into San Francisco for lunch and shopping at Fisherman's Wharf. That evening there will be a no host cocktail and dinner welcoming party at the Event Center. Saturday will be the train sales followed by the banquet and our guest speaker Gary Jarvis, Erector Expert. He will have with him spectacular Erector displays that boggle the mind. For details about The Fest, contact Jake Jacobsen at nut4aftrns@aol.com

May 19 and 20, 2017 — Allentown, Pennsylvania

23rd NATIONAL MODEL TROLLEY MEET at the Allentown Fairgrounds Agri-Plex, Charles Hall, 17th and Chew Streets. Largest Trolley modeling event in North America. Info at <http://easternpenn.org/meet2017.html>

May 20, 2017 — San Antonio, Texas

TCA 40th ANNUAL SPRING TRAIN SHOW at the Garden Ridge Community Center, 9500 Municipal Parkway. 10:30 am to 4 pm. Adults \$6 or \$9 per family.

May 20 and 21, 2017 — Jefferson, Texas

JEFFERSON TRAIN DAYS AND MODEL TRAIN SHOW Operating layouts, vendors, clinics, steam train rides, Jay Gould's private car. www.JeffersonTrainDays.com

May 25, 26 and 27, 2017 — Santa Clara, California

O Scale West /S WEST 12 is the largest S scale meeting in the west. Clinics, layouts, dinner, model contest, vendors. Info at <http://www.oscalewest.com/>

June 3, 2017 — Manassas, Virginia

WASHINGTON & OLD DOMINION S GAUGERS will display and run their layout at the **MANASSAS HERITAGE RAILWAY FESTIVAL** 9201 Center St., Old Town, Manassas.

June 3 and 4, 2017 — Omaha, Nebraska

NE-IA RAILROADERS 46th ANNUAL TRAIN & TOY SHOW at the Ralston Arena, 7300 Q St. 9 am to 4 pm.

June 7 through 10, 2017 — Spokane, Washington

SPOKANE FALLS EXPRESS 2017 the **Annual Pacific Northwest Convention** at the Red Lion at the Park. Info www.pnr5d.org/index_spokanefallsexpress2017.htm

June 11, 2017 — Wheaton, Illinois

CHICAGOLAND ASSOCIATION OF S GAUGERS will have their layout set up at the **Great Midwest Train Show** at the DuPage County Fairgrounds, 2015 Manchester Road.

June 16 and 17 — Emmett, Idaho

THE ROCKY MOUNTAIN HI-RAILERS will display their **LOOKOUT JUNCTION** layout at the **Emmett Cherry Festival** 107 East Main Street.

June 23 and 24, 2017 — Collinsville, Illinois

RAILROAD PROTOTYPE MODELERS MEET at the Gateway Convention Center. (Near St. Louis)

June 24 and 25, 2017 — Crockett, California

CARQUINEZ MODEL RAILROAD SOCIETY and the **CARQUINEZ TOY TRAIN OPERATING MUSEUM** will hold an open house and show at 645 Loring Avenue (Across from C&H Sugar). Info at www.cmrstrainclub.org

July 30 to August 5, 2017 — Orlando Florida

NMRA 2017 NATIONAL CONVENTION at the Rosen Plaza Hotel, 9700 National Drive. For more information go to the convention web site www.nmra2017orlando.org

August 3rd to August 6, 2017 — Orlando, Florida

NMRA NATIONAL TRAIN SHOW NASG will have a booth at this show. Volunteers are needed to staff the booth. To volunteer contact **Will Holt**, NASG Promotions Coordinator at willowassociates@comcast.net

August 8th to 12, 2017 — Baltimore, Maryland

NASG 2017 NATIONAL CONVENTION presented by the **Baltimore American Flyer Club** and the **Washington & Old Dominion S Gauge Club**. Up to date information can be seen at www.baltimoreamericanflyerclub.org

September 21 through 23, 2017 — Indianapolis, Indiana

The **S SCALE MIDWEST SHOW** in conjunction with the **INDIANAPOLIS O SCALE SHOW** at the Wyndham Indianapolis West. Web site: www.indyoscaleshow.com

September 23, 2017 — Boise, Idaho

The **ROCKY MOUNTAIN HI-RAILERS** will display their **LOOKOUT JUNCTION** layout at the **Museum Comes to Life** festival at the Julia Davis Park.

CLASSIFIED ADS

ANY ACTIVE MEMBER of NASG can place a non-commercial classified ad in the Dispatch for no charge. Ads must be limited to 10 lines+-. E-mail your ad copy to dispatch@nasg.org or mail to Bill Pyper, Editor, PO Box 885, Salem, OR 97308-0885. Please be sure to include your **member number**. Ads will run for three consecutive issues unless told to stop.

FOR SALE: SHS 00371 MEC #2 F3 A and B set. MAINE CENTRAL green and gold color scheme. Never operated. Configured for LocoMatic control with hi-rail wheelsets and couplers (LocoMatic 10-button control box and LocoMatic transformer included). No scale wheelsets. Owner's manual not included. \$500 which includes shipping, handling, and insurance. Call (262) 549-9405 with any questions.

FOR SALE: 48262 TTOS Union Tank \$50. 48292 TCA Scaramento Tank \$60. 48265 TTOS Life Savers Tank \$125. 48296 TTOS Wint-o-Green Tank \$70. 48298 TCA Spreckles #1 \$60. 48299 TCA Spreckles #2 \$60. 41020 TCA Spreckles #3 \$60. 41024 TTOS Smokey Bear Gondola \$65. 48249 TCA Yule Marble \$80. US Mail extra. Philip Capion, 30190 Apple Grove Way, Flat Rock, MI 48134-2750. Telephone 734-377-3252.

FOR SALE OR SWAP: SHS cars. All are new or mint in the box w/AF couplers and both wheelsets. \$35 each or \$30 each for more than one, plus shipping of your choice. **NYC offset hopper** in red #4 #01118 (#861784). **New Haven PS-2** #00080 (#117093). **Maine Central PS-2** #00030 (#2492). Also at same price, **D&H panelside hopper** set, NIB, \$100 OBRO. Contact jacekahn@hotmail.com or call 507-696-7700.

FOR SALE: (1) All-original AF 1957 NP Vista Dome North Coast Ltd set 20370. Mint. Item boxes (minor box damage). PA 492(21552)-roar, PA 490(21556)-horn, combine 24702, coach 24705, dome 24708, obs'n 24712, 708 horn control. \$1850 full set. (2) AF 918, red mail, new, nearly mint, box, liner, arm, rail, button, bags. \$110. (3) Original, mint, Hoquat GE 40t Steeplecab Electric kit (c1981); needs Athearn SW1500 HO chassis. US\$50. (4) NASG 1980 20th anniversary commem. AF gondola body, black, white decals. Never used. No trucks or couplers. \$20. For more info/photos/other items, email Bruce.Shore@sympatico.ca

FOR SALE Large collection of 3-rail O-gauge. Lionel, MTH, Williams, Weaver locomotives and rolling stock. Miles of used Lionel track and switches, unique Marx track and switches. E-mail rrbill10@comcast.net for a pdf or hard copy.

FOR SALE House of Trains S gauge wood 42 ft. reefer kit. Kit includes metal accessories. Early 1950s manufacture. Have 4 kits for \$20 each. Call 718-343-5875 after 7 pm EDT.

Side Track

Your S-Gauge Headquarters
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PO Box 590 • Ouray, Colorado 81427 • 970-325-0118
sidetracks_1@msn.com
www.sidetracks.net

FOR SALE: American Flyer Electromagnetic Crane #583, \$135. American Flyer Directronic Rectifier #15, \$25. Prices plus shipping. Robert Hartzell, 1960 Linden Lane, Whitehall, PA 18052. Cell phone 610-570-7121.

ESTATE SALE: Noted Canadian S-scaler Dan Kirlin died last summer. His model railroad equipment is for sale. If you are interested go to www.dansmodeltrains.ca

WANTED Any 4-8-4 American Flyer S-gauge or O-gauge. Any smoking tenders, American Flyer. Lionel GG-1 Brunswick Green. #14 and Bulb Transformer. Directional rectifier DC. Contact: Philip Peschi, 7 East Miami Ave., Villas, NJ 08251.

WANTED: 1/64th truck models. ERTL promo labels for grocery, hardware, gasoline tankers, Hartoy, LLEDO gas trucks, Road Champs, Ralstoy, Winross. All new or used. D. Griebel, PO Box 6602, Sherwood, AR 72124. Phone 501-541-4508.

WANTED: Will pay \$10 for **NASG old standards track gauge** from 1970 (1.05 square). Steve Monson, 4000 Willow Road, Brooklyn Park, MN 55443. monson@waves.space.umn.edu

WANTED: Trolley or "Doodlebug." S gauge. DC Current, Contact George at micha540@optonline.net

The **ROCK ISLAND REPORTER** is a free e-mail newsletter. To sign-up go to <http://eepurl.com/Ut8UL> Please pass this link to anyone interested in the Rock Island. Back issues available. E-mail updates appreciated. Stories and small articles are welcome. Send to Editor Tom Brugman, tbrug@aol.com

ANY MEMBER CLUB that has special edition cars left over from conventions or fests can advertise them at no charge in The Dispatch. Send a hi-res jpeg photograph and information to dispatch@nasg.org

SOHIO TANK CAR IN S GAUGE. The SOHIO Tank Car was made in HO by Lionel but never in S gauge. SOHIO stands for Standard Oil of Ohio. These Lionel 6-41026 cars sell for \$70 each which includes shipping and handling. To order make a check payable to **SCSGC** and mail to: Jeff Kruger, 1865 Nottingham Lane, San Dimas, CA 91773-3718



MONSANTO TANK CAR

We have extra cars from the **Fall S Fest 2015**. The car is the Monsanto Chemical Tank Car produced by Lionel as AF 6-41025. It is available only as hi-rail. The car may be obtained from AFSGLA for \$95⁰⁰ which includes shipping and handling. Make checks payable to **AFSGSLA** and send to: Moe Berk, 39 Graeler Drive., St. Louis, MO 63146-4938



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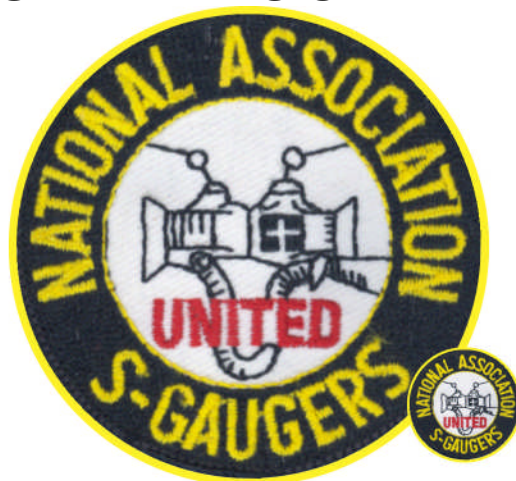
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Each year since 1981, the NASG contracts with one of the S manufacturers to produce a limited-run car or engine specifically geared toward the collector or American Flyer operator. Since this is an official NASG sponsored project, you must be an active NASG member to purchase one of these cars. This project is coordinated by Dave Blum, owner of Pikesville Models.



2016 NASG American Flyer Commemorative Car is a CNW Flatcar loaded with pallets of plastic bonded refractories headed for a steel mill for the relining of the soaking pits, or some other industrial use. Manufactured by Lionel, road number 3516 for the 35th edition and the year 2016. **Price \$79.** Extra pallets can be had 4 for \$10, decorated or undecorated



2015 NASG American Flyer Commemorative Car BNSF waffle side boxcar, manufactured by Lionel, road number 3415 for the 34th edition and the year 2015. Price \$69



2010 Jenney Gasoline Triple-dome Tank Car by Lionel. Road number 2910. Price \$75



2015 Convention Car Kansas City Southern PS2 Hopper car made for NASG by MTH. Available in two road numbers 286707 or 286815. Hi-Rail only for \$68



2012 Marathon Motors Boxcar. Road number 3112. Price \$75

TO ORDER THESE CARS

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Please include the following in your correspondence:

1. Your NASG ID number from your membership card.
2. Your name and shipping address.
3. Which car or cars that you want and how many of each.
4. Your telephone number or e-mail address in case there are questions about your order.
5. Maryland residents add 6% sales tax to your order.

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TO SHOP ONLINE

Go to www.nasg.org/Store/AFCars.php for the AF cars or www.nasg.org/Store/ConvCars.php for Convention Cars and click on the **Add to Cart** button for the item(s) that you want and follow the instructions.

2006 General Electric MOW Crane Car manufactured by Lionel. Price \$70*

2007 General Electric MOW Boom Car manufactured by Lionel. Price \$52*

***SPECIAL OFFER!** Buy both the Crane Car and the Boom Car for \$115



Convention Clinics

Continued from page 23

Learn about T-TRAK for S
Saturday 9-10 AM Chesapeake 1

The modular model railroad system based on a few standards for module size, track placement, track interface, and electrical connections. The minimal standards allow for a wide range of flexibility in design yet still maintain interoperability with all modules built per the standards. The popularity of T-TRAK is worldwide allowing for modules from all over the world to connect together. Yes, it does work in S.

The Automated Train Controls,
By James Ingram


Saturday 11 am to 12 noon, Chesapeake Room 1

This Automated Train Controls clinic will show and discuss methods of automatically controlling multiple trains on the same track. This control task consists of 2 parts: 1) the Logic and 2) the Hardware; and both will be discussed. This will include control of AC trains, DC trains, and DCC trains — the latter using DCC-Bitswitch and the NMRA-compatible NCE Mini-Panel automatic DCC train controller. This will include both single-track layouts (no turnouts), and use of automatic passing sidings where a pair of turnouts are used. Discussion of sensors will include magnets and reed switches, and electric eyes. The Anderman method of using automotive relays to operate the current-hungry original American Flyer turnouts will be demonstrated. The **AutoControls.org** automatic train control web site demonstrates all these various techniques in S gauge, Largescale, and HO gauge — while this clinic will focus on using them for S gauge.

More Clinics Wanted . . .

We would like to add a few more clinics to this list. A few of the subjects that have been requested or suggested are: MTH's DCS System, Installing DC Motor and DCC Controller in AF Steamers, How to install and wire a DCC controller and speakers in the same engine, How to build custom track work in code 125, How to kitbash a Lehigh Valley or other wooden kit, How to do airbrushing on model railroad cars.

If you are ready, willing and able to present these or any other clinics please contact David Blair at wdavidblair@verizon.net

Please check for additions, updates or changes to clinics on the convention web site, baltimoreamericanflyerclub.org
If you have questions go to **Contact Us.** 

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END OF THE LINE

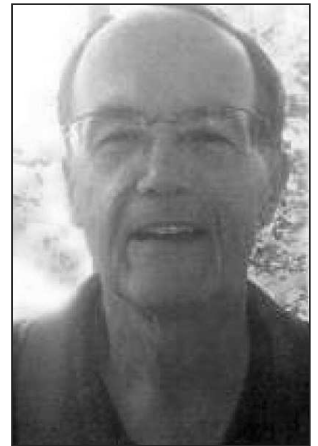
David K. Daniels, 86, of O'Fallon, Illinois, was born in Belleville, Illinois, and died Monday, January 30, 2017.

David collected Gilbert American Flyer toy trains, Erector sets, and other toys. He was one of the founders of the **American Flyer S Gaugers of the St. Louis Area** and helped design and build the Famous & Barr Christmas Train window display in downtown St. Louis.

He was preceded in death by his wife of 59 years, Genevieve; his parents, John and Leona Daniels; and his brother, Floyd Daniels. He is survived by his daughter Rhonda Brackett, of Eureka, Missouri; his sons David and Duane of O'Fallon, Illinois, and Kenneth of Fairview Heights, Illinois; and eight grandchildren and four great-grandchildren. 

Thomas (Tom) Riddle,

of Richmond, Virginia, died on February 16, 2017 from a heart attack. Born in Lynchburg, Virginia on November 15, 1951, he was 65 years old. Tom was very active in S-scale, involved in the TCA, an American Flyer collector, and an scale modeler. He was a member of NASG from 1976 to 2015 and a member of the Potomac Valley S Gaugers. Tom wrote for the S Gauge Herald magazine for many years as the "Riddle of S Gauge". 





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Member Dues

As of April 1, 2016, the yearly member dues are as follows:

\$18.00 One-time 6-month Trial Membership

\$30.00 Regular

\$40.00 Family

\$50.00 Contributing

\$50.00 Non-personal (Club, Business, etc.)

You can now **renew your membership online.**

Go to the web site www.nasg.org and click on membership. Under the heading **NASG Membership Renewal** click on renew and pay online. Enter your name and member number and follow the instructions. Have your credit card ready.

You can still renew your membership by sending a check or money order payable to NASG, to Dick Kramer, Membership Secretary, P. O. Box 268, Middlebury, CT 06762-0268.

NEW MEMBERS

From January 1 to February 28, 2017 the following people have joined the NASG. Welcome!

William Barrett Belmont, North Carolina	Sean Mummert Rock Hill, South Carolina
James W. Bittner Necedah, Wisconsin	Dennis Murphey Ashburn, Virginia
Donald L. Dobbs Mission Viejo, California	John Park Tyendinaga, Ontario, Canada
Braylon Gonya-Hubing Hartford, Wisconsin	Byron P. Peebles West Chester, Pennsylvania
Joseph E. Gornik Liberty Township, Ohio	David L. Petersen Wakeeney, Kansas
Jim E. Grace Locust Grove, Virginia	Jerry Smith Arnold, Maryland
Craig S. Harper Whittier, California	John S. Turek Nanticoke, Maryland
Brian W. Hevesy Dallas, Georgia	David A. Verwys Grand Rapids, Michigan
Robert (Bob) A. Haon Marietta, Georgia	*****
John L. Jakubowski Niagara Falls, New York	Deceased Members
Philip H. Kehr York, Pennsylvania	David K. Daniels O'Fallon, Illinois
Ted Konfederak Apache Junction, Arizona	Arden Goehring Lodi, California
David A. Lee Eureka, California	Tom Riddle Henrico, Virginia
Clifford H. Merkle Marilla, New York	Don Stratton West Covina, California

EXTRA PAGES ONLINE

Go to www.nasg.org to see more pages.

Click on the **NASG** button. Click on the **Membership** menu option. Enter your **member number** and **password**. Click on **Recent Dispatch Issues**. Use the **drop down menu** to find this issue. Click on **Retrieve**, then **Click here to download or view** the requested Dispatch. Enter Password. When magazine appears, scroll to page 33.

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S Scale Budd Company Vista Domes

New: LED Lighting



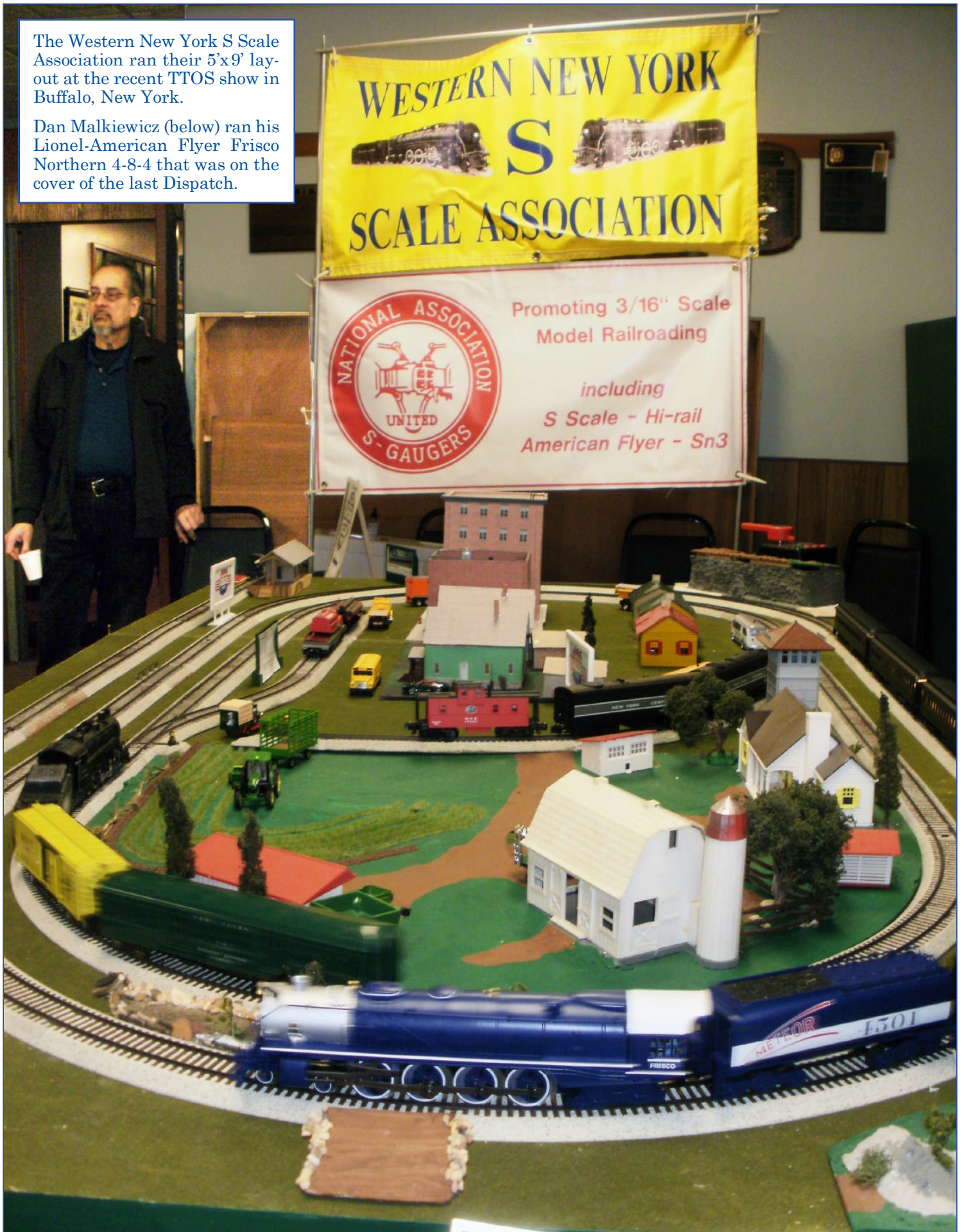
Announcing targeted shipping in September: Budd Vista Dome cars. Probably the most requested item for the last 10 years. We will have all the road names (save the IC) we made our Budd sets in (including the chrome cars). Features: full interiors, LED lighting upstairs and down, cast metal frame and metal composite trucks with all wheel electrical pick up for non-flickering lighting. Added grab irons and details. Introductory priced at \$109.95 or 3 for \$299.95 All three would have different printed names or numbers on the cars. Call now for reservations.



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The Western New York S Scale Association ran their 5'x9' layout at the recent TTOS show in Buffalo, New York.

Dan Malkiewicz (below) ran his Lionel-American Flyer Frisco Northern 4-8-4 that was on the cover of the last Dispatch.



RAILROAD MUSEUM of S. FLORIDA



BAY AREA S SCALERS LAYOUT

The Southern Loop

By Michael Eldridge, Photos by the author

In the last issue of The Dispatch I introduced the new Free-Mo style modular layout being built by the Bay Area S Scalpers. This time I will focus on the Southern Loop, which I am frantically trying to get ready for S-West at the end of May. This may come as a shock to some of you, but I haven't gotten as far along as I would have liked to by now. Just me, right? We can still take a quick tour.

The track plan is fairly simple. There is one run-around track and two sidings. See the diagram on the right. Photo 1 below shows us entering the loop from the rest of the layout. The end nearest the camera is the only end that has to meet the module standard, since this whole loop is one module made of seven tables. The track is hand laid, yet to be painted, and you can see some of the track detailing at the switch. Photo 2 shows a farm scene. I put this farm in the wye because it draws your eye away from the other part of the loop, making it less obvious that in just a minute we are going to come back to this wye.

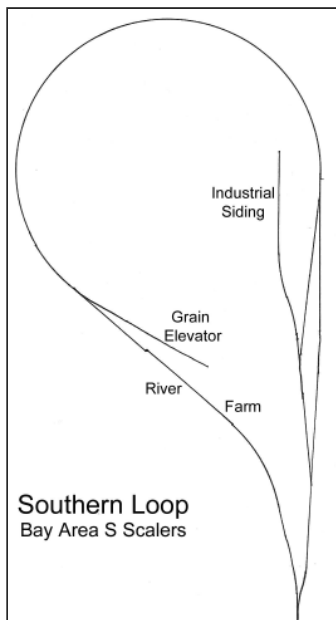


Photo 1



Photo 2



Photo 3

Photo 3, above, shows the river. Here we need to think of Back To The Future Part III "You're not thinking fourth dimensionally!" — the bridge will be there in the future, so just keep going. The drought is over here in California, so I guess I will need to put some water in the river. Photo 4, below, shows a grain elevator and feed supply company that won the kitbashing award in the November 2016 Railroad Model Craftsman. I will end up scratch-building this structure, but a photo mock-up will have to do for now. In Photo 5 on the next page, we are going through the curved turnout. This was a last-minute change in design, but it allowed me to add an industrial siding. I had to figure out how to hand lay a curved turnout, but it turned out to be no more difficult than a straight turnout. In Photo 6 we are in the actual loop, and then in Photo 7 we are leaving the loop via the wye.



Photo 4

One thing that I thought would be of interest is the electronics hiding under the wye. The module specification says that each turnout can be controlled locally from either side of the module. I also wanted the loop to work without any operator intervention. The loop power is fed from a DCC Specialties PSX-AR. This as an automatic reversing circuit, that also provides short circuit protection. Its main use is to switch the polarity in the loop when the train crosses back into the wye, preventing a short circuit and simplifying the loop wiring.

Continued on the next page

BAY AREA S SCALERS LAYOUT



Photo 5

Going through the curved turnout.



Photo 6

In the loop.



Photo 7

Leaving the loop via the wye.

The PSX-AR has two other features I didn't know about. First, it can automatically throw the switch motor to align the switch when entering from the frog end. There is a kludge circuit to also add right and left buttons, but we had specified one-button control for the turnouts. We also want to have animation on the layout, so I wanted to have signals at the wye.

It occurred to me that this might be a good application for a microprocessor based circuit. Once you make that decision, you just start thinking of what you would like the circuit to do, with the assumption that whatever you want to do will be possible. This led to a set of design goals:

- When no train is in the loop, the switch can be thrown from a button on the side of the module. There will be one button on each side, and pressing either will toggle the turnout position.
- When no train is in the loop, the signal visible from the point end will show green for the selected route, red for the other. Signals visible from the frog end will show red.
- When a train enters the loop, all signals will go red. After the train has cleared the wye turnout, the signal viewable from the frog end will go green for the route where the train is expected to exit. When the locomotive passes this signal, it will go red.
- When a train is in the loop, the buttons on the side of the module will be disabled. I don't want operators to be tempted to throw the switch at the wrong time.

This requires that we know when a train is in the loop. Fortunately, the PSX-AR also has a current based occupancy detection output that can go to the microprocessor. The limitation, of course, is that current detection requires resistors on the cars — something not really feasible for the loosely organized club we are. I decided to put optical detectors near the frog in each route. In retrospect, it probably would have worked just as well to put one optical detector in the approach near the points, but having two may come in handy if I need to change how the signal logic works. The microprocessor can operate the LED and detector for the optical detection, so the cost is under two dollars for each route.

I will use an Arduino processor for all of this. The hard part of using a microprocessor is being able to specify exactly what you want it to do. Once you know that, programming something like the Arduino is relatively easy. The good part is that you can change your mind later, and reprogram the same Arduino with the new design.

At this point I am beginning construction of the through truss bridge that will span the river. This is an accurate model of a bridge spanning the San Lorenzo River in Santa Cruz, California. That's a lot of rivets! I will probably only finish the deck — enough to run a train over it — by the S-West convention. As you can see, I still have a considerable amount of track to lay and some scenery that needs to be done for several tables.

I hope you will come see all this at O Scale West and **S-West 12**, May 25-27 at the Hyatt Regency in Santa Clara, California. Bring an S model for the model contest — at least one S scale model has won its category each year I've attended this convention. 🚂



Cindy Chinn: Artist, Painter, Muralist, Creator

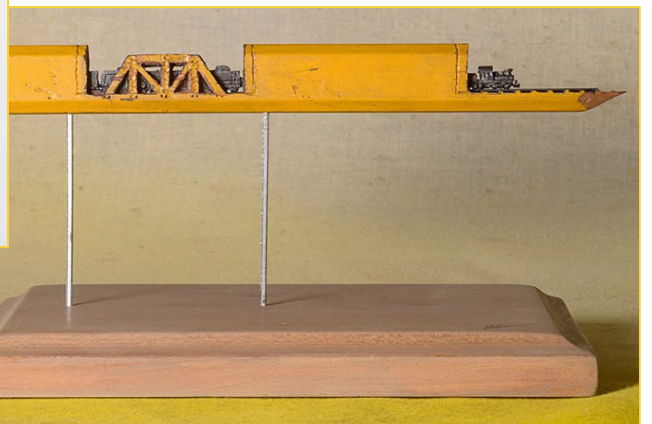
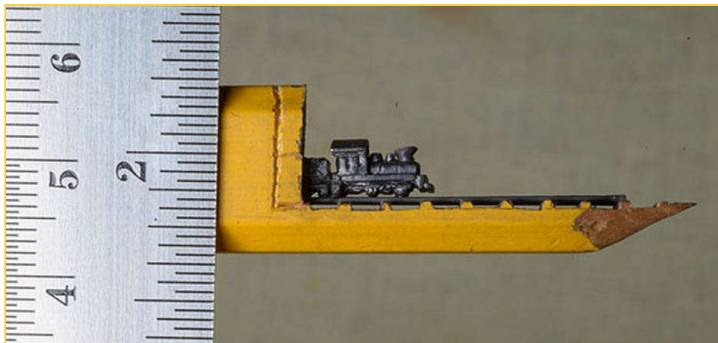
There's a lot of cool artwork that I see every day. I'll spend some time at the start of my day seeing what my 'Facebook Friends' are working on and what interesting things they are sharing. This exposes me to a lot of new media and techniques.

One medium that has always intrigued me is the art of carving pencil leads. I've always loved carving and sculpting and seeing the tiny works of art created from a such a common item is fascinating!

I finally found some time and went to work. I bought some carpenter's pencils and tried a number of different subjects. Some worked better than others, and some were just a little too hard to get the detail I needed to pull off in such a small space.

In December 2015, a photo of my first train pencil carving went REALLY viral on Facebook and since that time, I've been known internationally as that 'pencil artist from Nebraska'.

Here are some pictures of one of my creations. I hope to continue with these as they're fun and don't take a lot of time compared to a painting or other major works. Go to my web site www.cindychinn.com to see more of my railroad pencil carvings or contact me if you have any questions or like to commission your own custom pencil carving!



MORE POULTRY CARS

By Bill Pyper, Editor, NASG Dispatch

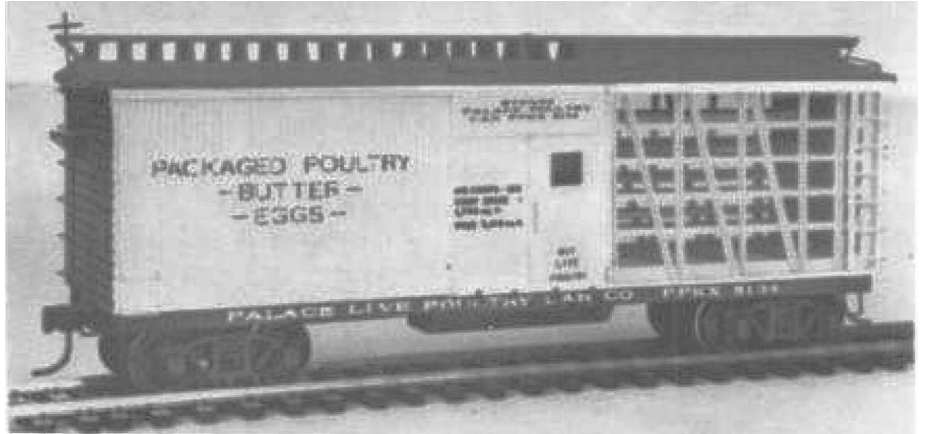
Ever since I published **Turkeys by Train** in the November-December 2016 Dispatch, I have been hearing from members asking why I didn't know of these kits. The answer is simple; I have only been involved with S gauge since 2002. These kits were produced long before that. I took on the job of editor of the Dispatch, not because of any extensive knowledge of S, but because of my expertise in producing magazines.

Pictured below is an excerpt from the Lehigh Valley Models catalog and below that is an ad for the Main Line Models kit that was printed in the S Gauge Herald in the 1960s.

On the following pages are some pictures that David Tyner took as he was building his Lehigh Valley model. Following that is an article by Ed Kirstatter describing how he built his LVM Poultry Car. It had been sitting on his shelf for years, and my article motivated him to build it.

LVM45 - PALACE POULTRY CAR -

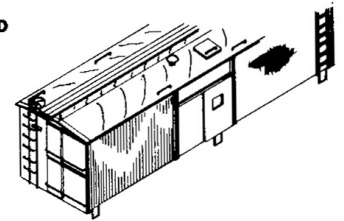
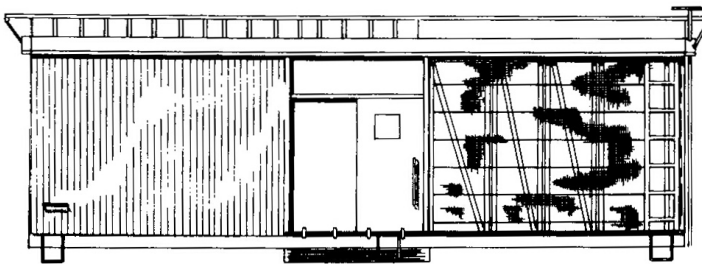
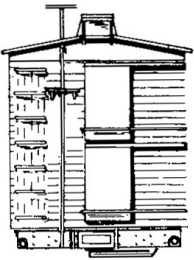
These cars were built to ship live poultry which was kept in 120 coops. Hinged trapdoors in the car roof were for access to the watertank and also for additional ventilation during the hot weather. The coops were stored on shelves in the screened area. The attendant lived in the enclosed area above a large feed bin. A green and white conversational piece. Easy to build. Specify Scale or Hi-Rail.



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SPC-3 MUDD BLACK ROOF, BLACK ENDS, UNDERBODY

SPC-2 NATIONAL POULTRY BOX CAR RED ROOF AND ENDS BLACK UNDERBODY

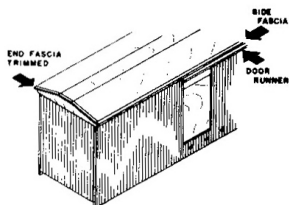
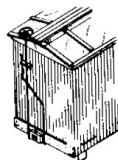
SPC-4 LEGHORNS LPTX BOX CAR RED ROOF CABOOSE RED ENDS BLACK UNDERBODY

WOOD BOX CAR

TRUSS RODS

- ___SBC-40 MENASHA WOODEN WARE 51'3" GREEN
- ___SBC-41 RACINE WAGON & CARRIAGE CO. 51'3" BOX CAR RED

LESS TRUCKS & COUPLERS \$8.75



DEALER & MAIL ORDERS INVITED

SIDES, DOORS AND ENDS SILK SCREENED LETTERED

- ___SBC-42 QUAKER OATS 37' YELLOW
- ___SBC-43 BALL LINE 37' YELLOW
- ___SBC-44 C.H. & D. 37' BOX CAR RED
- ___SBC-45 PENNSYLVANIA 37' BOX CAR RED
- ___SBC-46 N.Y.C. & HUDSON RIVER 37' YELLOW
- ___SBC-47 SOO LINE 37' BOX CAR RED
- ___SBC-48 TOLEDO & WESTERN 37' BOX CAR RED
- ___SBC-49 C.M. AND St. P. 37' BOX CAR RED
- ___SBC-50 BASIC KIT 37' UNPAINTED

LESS TRUCKS & COUPLERS \$7.95

MAIN LINE MODELS

P.O. BOX 371
MADISON, VIRGINIA 22727

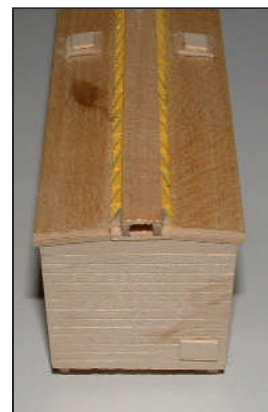
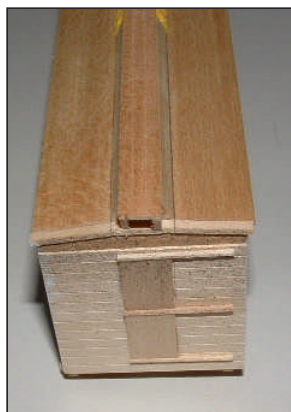
MORE POULTRY CARS

From David Tyner . . .

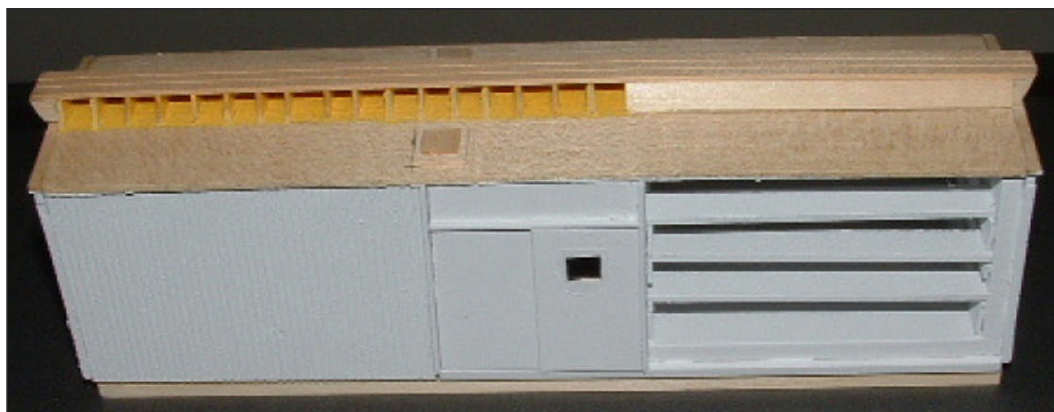
I just read through the latest Dispatch and noticed that you could not find any poultry cars in S. Lehigh Valley Models made an S-gauge poultry car kit #LVM45. I've attached a photo of the one I assembled and turned into a chicken car. I can pull it out of the box and send more photos if you'd like. Here are a few photos of some of the assembly steps from my electronic files.

Run 'em if you got 'em,

David Tyner



There were no chickens included with the kit so we hand made some. At the time I built the car the Yahoo S-Trains group was very active. Paul Yorke sent me a 1/64 scale chicken and I made a two-part mold. I used modeling clay to make each chicken half and we glued them together, then Tracie painted them. Not the best chickens ever made as the halves didn't match up very well, but they'll do. It was a fun project to get into something we had never tried before.



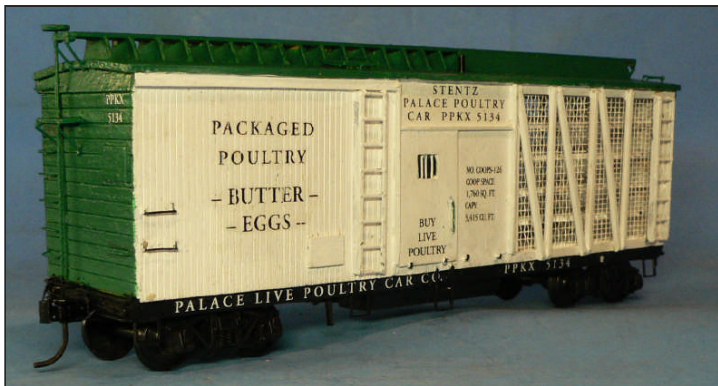
A POULTRY CAR AS AN S MODEL

By Edwin Kirstatter, Photos by the author

When I saw Bill's article in the Dispatch I wondered why he didn't know about the Lehigh Valley Models Palace Poultry Car kit #LVM45, a wood craftsman's kit?

That company was run by Frank Titman and a group of S modelers from Allentown, Pennsylvania. They made many freight cars and structure kits. These could all be seen on Franks Spiral Hill layout. No I didn't scratch build one as Bill suggested, but I took mine down off the shelf and built it. It took me a month to finish. It is almost scratch building.

This is a combination car, one end is for live poultry, the other end is refrigerated for packaged poultry-butter-eggs and the middle is the living quarters for the Poultry-man attendant. I am not sure I would want that job? To feed and water those Birds in route! It is a 36' car of an AAR Class SPR.



This wood kit is a very complicated one to build even with most of the wood pieces accurately cut to size for you. One pair of Kadee #802 couplers are included, ACE Arch-bar trucks as a kit, a KC brake white metal casting, Northeastern's formed grab irons, .020" wire and custom printed decals just for this car. But we have better adhesives now!

The instructions are as big as four sheets of paper printed both sides. Very good for the most part with text and several illustrations on how to do things and with a plan of this car in full S scale.


I like to jump around as I assemble a kit like this, I do sub-assemblies. I like to do the Underframe first. They included only a casting for a KC type of brake. To this I added complete air brake pipes, brake levers and rods. I added channeled corner braces and end sills, polling pockets and cut-levers of the top operating type and the missing tack boards. Then I painted that structure black.

The roof was done next to include all of the many parts on it then it was painted Green. The body was done next including the under frame, sides, ends and roof. While cutting out the side doors windows I added bars behind them as seen on the HO models. I added A-line weights inside. His instructions are incorrect for the placement of the wire screen! It should be placed behind the Hat-sections side braces. This can be seen on the prototype car picture on page 11 of Dispatch. Paint these white, being careful not to get it on the black under frame or fill the screen. The ladders, grab irons, brake staff with a brass wheel in place of the crude one supplied and a retainer valve and its pipe were added last. Then add a second coat of paint. I used Tichy #1106 .0125" Phosphor Bronze wire instead of the .020" wire furnished for making the other grab irons, this matched the size of the Northeastern ones. I used Detail-Associates #2524 .010"x .030" flat brass for the running boards end braces and brakeman's platform.

They suggest that you paint this Palace Poultry car three colors. Black for the under frame, I used Scalecoat S2010 on that, White sides, I used Titanium White Craft Acrylic for that and Reading Green on the roof and ends. The trucks were already black and mounted with #4 x1/2" flat head wood screws with washers added to correct the height using an NASG coupler gauge. The wheels had to be re-gauged to NASG standards and blacken the axles.

Except for the black paint these others have a flat finish and should be glossed before decaling. I used Krylon Clear for this. The decals went on very well and fitted as they were supposed to, just as shown on the instruction sheet.

Then I put onto the tack boards the Car Tags! My car weighs 9 ounces. You could weigh all of the kits parts on a postal scale to see if it will meet NMRA recommended weights.

This model could be updated for a more modern use by using the Grandt Line 4057 Westinghouse AB schedule brake kit and some cast steel trucks like the American Models and couplers with bottom operating cut levers. 

References:

NASG Dispatch, Nov.-Dec. 2016 Pages 2, 5, 10-11.
1946 Car Builders' Cyclopedia. Pages 152, 155.
Official Railway Equipment Registers, ORER.
S Gauge Herald.